



PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS.....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES AND UTILITY OWNERS.....	2A
GENERAL NOTES & SPECIAL NOTES	2B-2B3
GUARDRAIL PLAN & PAVEMENT TRANSITION DETAILS (BR. NO. 30-SR34-17.25 L&R)	2C
TRAFFIC CONTROL PLAN BR. NO. 30-SR34-17.25 (LEFT & RIGHT)	3-E
TRAFFIC CONTROL PLAN SNAPPS FERRY ROAD	3F-3G
TRAFFIC CONTROL PLAN WIDE LOAD DETOUR.....	3H

THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS.....	1A
LAYOUT OF BRIDGES TO BE REPAIRED.....	BR-131-902
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES.....	BR-131-903
PHASE CONSTRUCTION	BR-131-904
SUPERSTRUCTURE REPAIRS	BR-131-905
ABUTMENT REPAIRS LEFT BRIDGE.....	BR-131-906
ABUTMENT REPAIRS RIGHT BRIDGE.....	BR-131-907
BENT REPAIRS BENT 1 – LEFT BRIDGE.....	BR-131-908
BENT REPAIRS BENT 2 – LEFT BRIDGE.....	BR-131-909
BENT REPAIRS BENT 3 – LEFT BRIDGE.....	BR-131-910
BENT REPAIRS BENT 1 – RIGHT BRIDGE.....	BR-131-911
BENT REPAIRS BENT 2 – RIGHT BRIDGE.....	BR-131-912
BENT REPAIRS BENT 3 – RIGHT BRIDGE	BR-131-913
REPAIR DETAILS.....	BR-131-914
CONCRETE REPAIR DETAILS	BR-131-915

YEAR	PROJECT NO.	SHEET NO.
2025	30S034-M3-002	BRIDGE-SIGN1
GREENE COUNTY		S.R. 34
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET BRIDGE NO. 30-SR34-17.25 (R & L) FEDERAL BRIDGE ID NOS. 30FA0343003 & 30FA0343004 BRIDGES OVER SNAPPS FERRY RD. & NFS RAILROAD, LM 17.25 GREENE COUNTY 2025		

INDEX OF SHEETS

SEE SHEET 1-A FOR
INDEX AND STANDARD DRAWINGS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

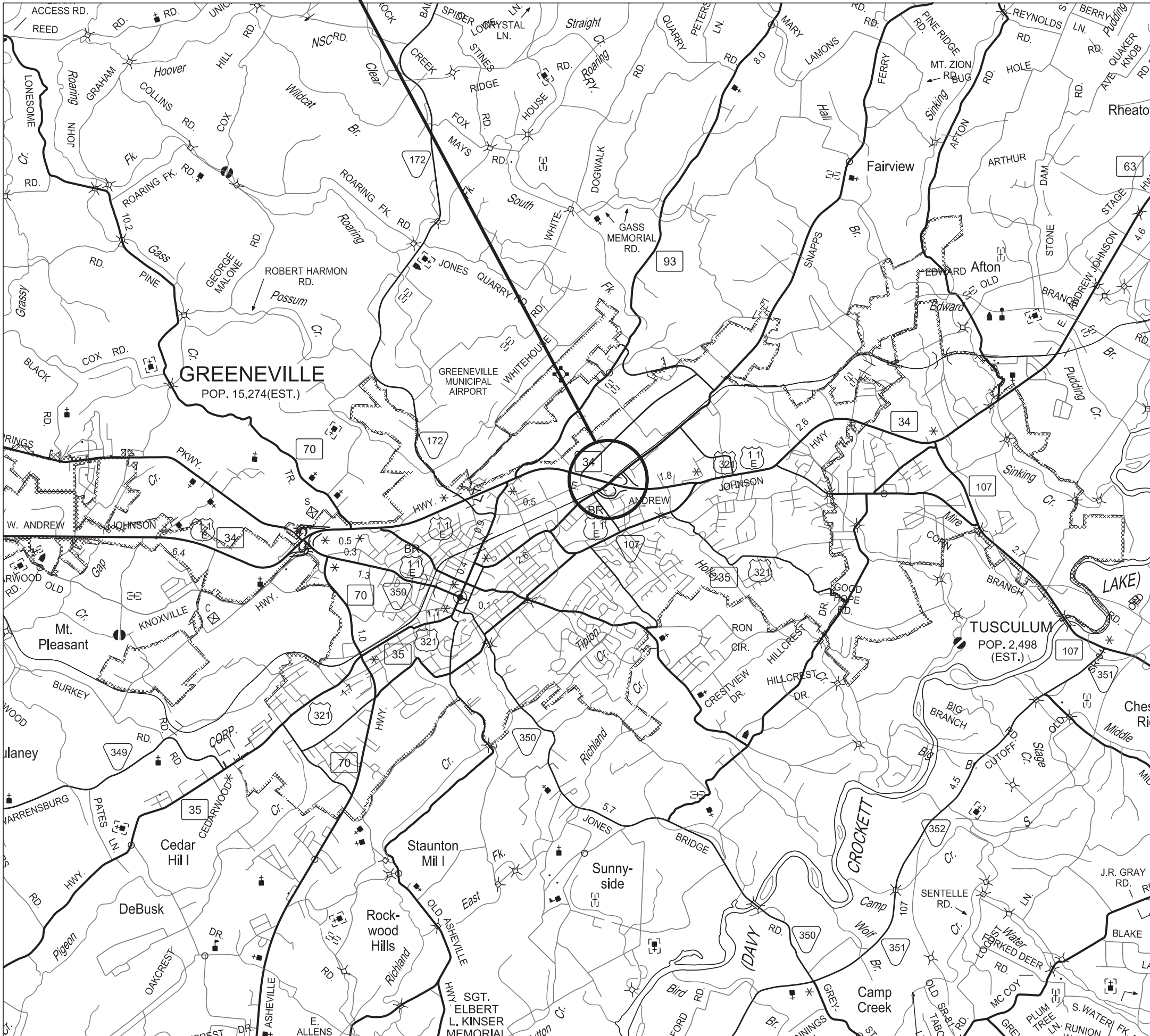
GREENE COUNTY

STATE ROUTE 34
BRIDGES OVER SNAPPS FERRY ROAD
AND NFS RAILWAY, LM 17.25

PS&E
BRIDGE REPAIR

STATE HIGHWAY NO. 34 F.A.H.S. NO. NA

BRIDGE NOS: 30-SR34-17.25 (R & L)



SCALE: 1" = 5,280' (1 MILE)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE PROJECT MANAGER STEPHEN WILSON

DESIGNED BY CDM Smith

DESIGNER SUSANNE DAWSON, P.E.

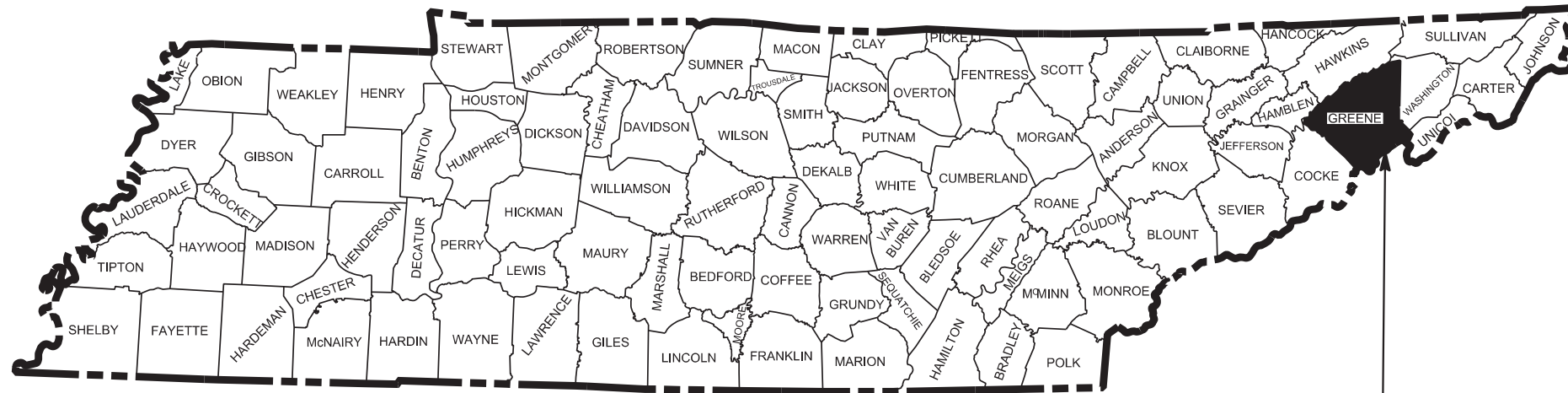
P.E. NO. 30S034-M3-002

PIN NO. 082699.01

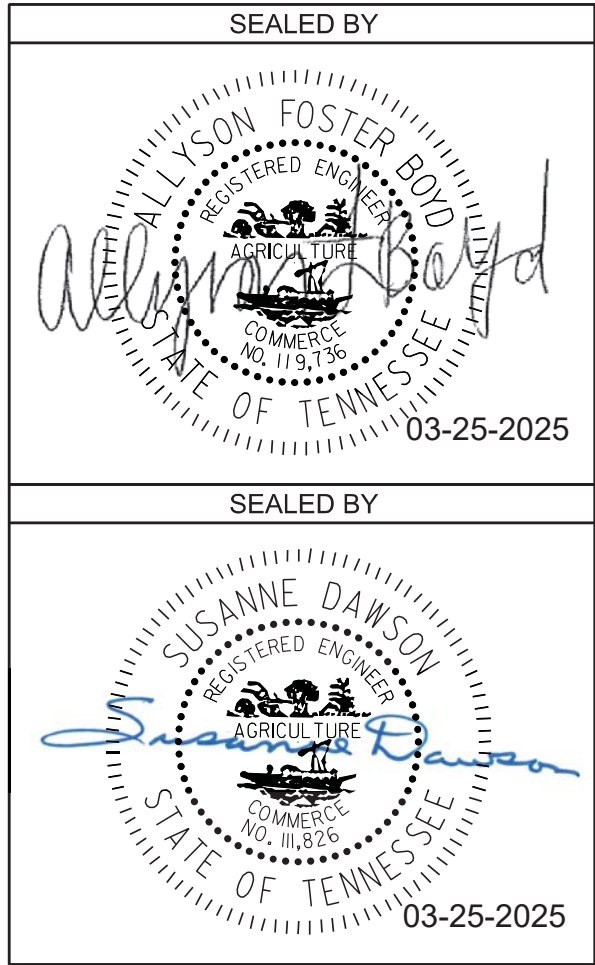
TOTAL DISTURBED AREA = LESS THAN AN ACRE

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	30S034-M3-002	
FED. BRIDGE ID NO.	30FA0343003 & 30FA0343004	



PROJECT LOCATION



TRAFFIC DATA
Snapps Ferry Road

ADT (2025)	6359
ADT (2045)	8903
DHV (2045)	979
D	51-49
T (ADT)	3
T (DHV)	3
V	40/45

TRAFFIC DATA
SR 34

ADT (2025)	30630
ADT (2045)	42882
DHV (2045)	3430
D	51-49
T (ADT)	5
T (DHV)	5
V	30

APPROVED: 
WILL REID, DEPUTY COMMISSIONER /
CHIEF ENGINEER

DATE: _____

APPROVED: 
HOWARD H. ELEY, DEPUTY GOVERNOR &
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME

SHEET NO.

SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET.....	1
INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES AND UTILITY OWNERS	2A
GENERAL NOTES & SPECIAL NOTES	2B-2B3
GUARDRAIL PLAN & PAVEMENT TRANSITION DETAILS (BR. NO. 30-SR34-17.25 L&R).....	2C
TRAFFIC CONTROL PLAN BR. NO. 30-SR34-17.25 (LEFT & RIGHT)	3-3E
TRAFFIC CONTROL PLAN SNAPPS FERRY ROAD.....	3F-3G
TRAFFIC CONTROL PLAN WIDE LOAD DETOUR	3H

STANDARD ROADWAY DRAWINGS

DWG. REV. DESCRIPTION

ROADWAY DESIGN STANDARDS

RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

SAFETY DESIGN AND GUARDRAILS

S-CZ-1	06-28-19	CLEAR ZONE CRITERIA
S-PL-3	03-01-23	SAFETY PLAN MINIMUM INSTALLATION AT BRIDGE ENDS
S-PL-5	06-28-19	SAFETY PLAN FOR BRIDGE ENDS IN MEDIANS
S-CC-1	10-01-24	CRASH CUSHION
S-GR31-1	06-15-21	GUARDRAIL DETAILS
S-GRC-4	01-30-25	GUARDRAIL CONNECTION TO BRIDGE RAILING CONCRETE PARAPET
S-GRC-5	02-28-20	GUARDRAIL CONNECTION TO BRIDGE ENDS (TRAILING ENDS)
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2P	10-16-20	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)
S-GRA-3	01-09-24	TYPE 13 GUARDRAIL ANCHOR
S-GR28-7M	06-28-19	GUARDRAIL ATTACHMENT TO BRIDGE END DETAILS

DESIGN - TRAFFIC CONTROL

T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-42	03-05-17	CENTER LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-PBR1	12-09-22	INTERCONNECTED PORTABLE BARRIER RAIL
T-WZ-PBR2	02-28-20	DETAILS FOR WORK ZONE CHANNELIZATION DEVICES

EROSION PREVENTION AND SEDIMENT CONTROL

EC-STR-3C	03-01-23	SILT FENCE WITH WIRE BACKING
EC-STR-3D	03-01-23	ENHANCED SILT FENCE
EC-STR-37	06-10-14	SEDIMENT TUBE

LIST OF BRIDGE DRAWINGS

DRAWING	DRAWING NO.
LAYOUT OF BRIDGES TO BE REPAIRED	BR-131-902
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES	BR-131-903
PHASE CONSTRUCTION	BR-131-904
SUPERSTRUCTURE REPAIRS	BR-131-905
ABUTMENT REPAIRS LEFT BRIDGE	BR-131-906
ABUTMENT REPAIRS RIGHT BRIDGE	BR-131-907
BENT REPAIRS BENT 1 - LEFT BRIDGE	BR-131-908
BENT REPAIRS BENT 2 - LEFT BRIDGE	BR-131-909
BENT REPAIRS BENT 3 - LEFT BRIDGE	BR-131-910
BENT REPAIRS BENT 1 - RIGHT BRIDGE	BR-131-911
BENT REPAIRS BENT 2 - RIGHT BRIDGE	BR-131-912
BENT REPAIRS BENT 3 - RIGHT BRIDGE	BR-131-913
REPAIR DETAILS	BR-131-914
CONCRETE REPAIR DETAILS	BR-131-915

LIST OF BRIDGE REFERENCE DRAWINGS

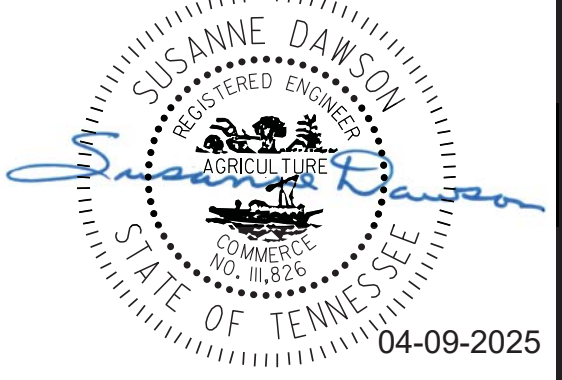
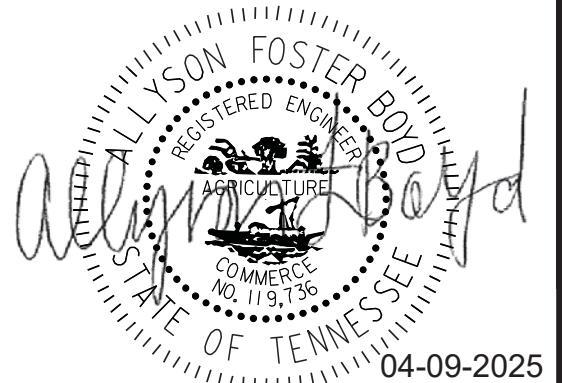
(TO BE PRINTED WITH PLANS)
BR-16-74, BR-16-76 THRU BR-16-81,
K-27-108 THRU K-27-112 AND K-15-65

STANDARD BRIDGE DRAWINGS

DRAWING NO.	LATEST REV. DATE	DRAWING
STD-8-5	05/10/2021	STANDARD PROTECTIVE FENCE DETAILS
STD-10-3	01/10/2024	STANDARD FLUME DETAILS

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	1A
PS&E	2025	30S034-M3-002	1A

GREENE COUNTY S.R. 34

SEALED BY

SEALED BY


STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
INDEX AND STANDARD DRAWINGS

4/9/2025 10:02:43 AM
c:\pw_pl1\picke\dp\04793899\001B_Project_Committments.dgn

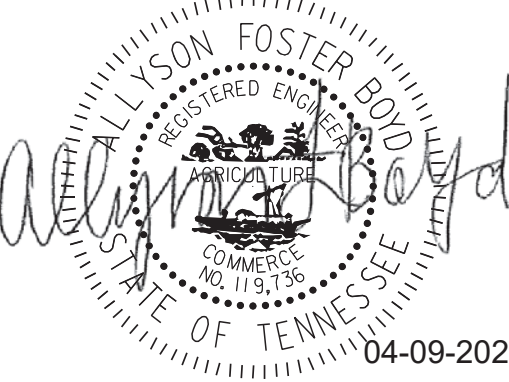
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	1B
PS&E	2025	30S034-M3-002	1B

GREENE COUNTY

S.R. 34

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	Asbestos Containing Material (ACM) surveys were completed on Bridge No. 30FA0343003 SR-34 EB over Snapps Ferry Road and NFSRR LM 17.25 (30-SR034-17.25R) and Bridge No. 30FA0343004 SR-34 WB over Snapps Ferry Road and NFSRR LM 17.25 (30-SR034-17.25L) and no ACM was detected. Please see the reports for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

4/9/2025 12:04:04 PM
c:\pw_pl1\pickel\p\04294646\02A_Est_Roadway_Quan.dgn

10
12
1 2
1

5
3
4 6
4 7
4
4
1 3 4
4

1
1

5
1 8
9
9 11

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
201-01	CLEARING AND GRUBBING	LS	1
203-06	WATER	M.G.	0.32
209-08.02	TEMPORARY SILT FENCE (WITH BACKING)	L.F.	150
411-02.10	ACS MIX (PG70-22) GRADING D	TON	65
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	977
705-06.10	GR TERMINAL TRAILING END (TYPE 13) MASH TL-3	EACH	2
707 08.11	HIGH-VISIBILITY CONSTRUCTION FENCE	L.F.	150
709-05.06	MACHINED RIP-RAP (CLASS A-1)	TON	387
712-01	TRAFFIC CONTROL	LS	1
712-02.02	INTERCONNECTED PORTABLE BARRIER RAIL	L.F.	950
712-02.60	TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	2
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	130
712-04.50	BARRIER RAIL DELINEATOR	EACH	48
712-06	SIGNS (CONSTRUCTION)	S.F.	2044
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	30
712-08.03	ARROW BOARD (TYPE C)	EACH	2
712-09.02	REMOVABLE PAVEMENT MARKING LINE (8" BARRIER LINE)	L.F.	13,220
712-09.31	REMOVABLE BLACK-OUT TAPE (8")	L.F.	8,000
716-12.02	ENHANCED FLATLINE THERMO PVMT MARKING (6IN LINE)	L.M.	0.3
717-01	MOBILIZATION	LS	1
740-10.04	GEOTEXTILE (TYPE IV) (STABILIZATION)	S.Y.	535
740-11.04	TEMPORARY SEDIMENT TUBE 20IN	L.F.	300
801-02	SEEDING (WITHOUT MULCH)	UNIT	3.2
805-12.01	EROSION CONTROL BLANKET (TYPE I)	S.Y.	354

FOOTNOTES

- 1
- ITEM MAY BE INCREASED OR DECREASED AS DIRECTED BY THE TDOT ENGINEER.
- 2
- ITEM TO INCLUDE THE COST OF 0.3 TONS OF BITUMINOUS TACK COAT (T.C) AT RATE OF 0.07 GAL/S.Y. (GENERAL USE) AND 0.10 GAL/S.Y. (COLD PLANE AREAS).
- 3
- THIS ITEM INCLUDES THE INSPECTION AND MAINTENANCE OF ANY SIGNING AND TRAFFIC CONTROL APPURTENANCES DURING THE CONSTRUCTION OPERATIONS.
- 4
- ALL COSTS ASSOCIATED WITH INSTALLING, STORING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES WILL BE INCLUDED IN THE BID ITEM OF EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
- 5
- FOR DRAINAGE DITCHES.
- 6
- ITEM SHALL INCLUDE COST OF INSTALLING AND RELOCATING PORTABLE BARRIER RAIL.
- 7
- THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED ON THE MANUFACTURER'S DRAWING.
- 8
- THIS ITEM INCLUDES ALL MATERIALS INCLUDING GEOTEXTILE FABRIC AND LABOR NECESSARY FOR CONSTRUCTION, MAINTENANCE, AND REMOVAL OF SEDIMENT TUBE.
- 9
- FOR DISTURBED AREA RESTABILIZATION.
- 10
- FOR SEEDING.
- 11
- THIS ITEM SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION AND MAINTENANCE OF EROSION CONTROL BLANKETS.
- 12
- INCLUDES THE COST OF ALL LABOR AND MATERIALS FOR FURNISHING AND INSTALLING THE TEMPORARY SILT FENCE WHERE LOCATED BY THE ENGINEER, AND REMOVAL UPON PROJECT COMPLETION. ALSO INCLUDES COST TO REMOVE SEDIMENT ACCUMULATION. SEE STD. DWGS. EC-STR-3C AND EC-STR-3E.

UTILITY OWNERS

ELECTRIC:
GREENEVILLE LIGHT & POWER SYSTEM
P.O. BOX 1690
GREENEVILLE, TN 37744-1690
ATTENTION: STACY BOLTON
sbolton@glps.net
PHONE: (423)636-6218; (423)832-0560

TELEPHONE:
CENTURYLINK
101 NORTH ROAN STREET
JOHNSON CITY, TN 37601
ATTENTION: ANDREW ICE
andrew.f.ice@centurylink.com
nationalrelo@centurylink.com
PHONE: (423)461-7724; (423)470-5636

SEWER:
GREENEVILLE WATER COMMISSION
P.O. BOX 368
516 N MAIN STREET
GREENEVILLE, TN 37744-0368
ATTENTION: ERIC FRYE
efrye@greenevilwater.org
PHONE: (423)638-3148; (423)823-1330

WATER:
GREENEVILLE WATER COMMISSION
P.O. BOX 368
516 N MAIN STREET
GREENEVILLE, TN 37744-0368
ATTENTION: ERIC FRYE
efrye@greenevilwater.org
PHONE: (423)638-3148; (423)823-1330

GAS:
ATMOS
2833 WEST MARKET STREET
JOHNSON CITY, TN 37604
ATTENTION: ISAIAH GREER/ SETH BRADBURN
isaiah.greer@atmosenergy.com
seth.bradburn@atmosenergy.com
PHONE: (423)202-1455; (423)440-4521

CABLE:
CHARTER COMMUNICATIONS – TRI-CITIES ENGINEERING DEPT.
10417 WALLACE ALLEY STREET
KINGSPORT, TN 37663
ATTENTION: MARK TAYLOR; DON RILEY; ADAM CLONINGER
mark.taylor2@charter.com
donald.riley@charter.com
adam.cloninger@charter.com
PHONE: (423)212-2349; (423)212-2344; (423)212-2364

FIBER OPTICS:
SUNSET DIGITAL COMMUNICATIONS, INC. dba/ POINT BROADBAND
15022 LEE HIGHWAY
BRISTOL, VA 24202
ATTENTION: NOAH MADEN/ TIM NUTTER
noah.maden@point-broadband.com
tim.nutter@point-broadband.com
PHONE: (423)631-1166

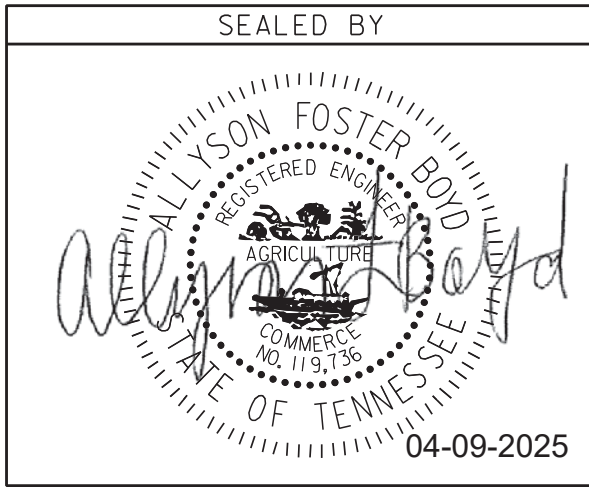
UTILITY NOTES

UTILITY

- (1)
- NO UTILITY CONFLICTS ARE ANTICIPATED BASED ON THE SCOPE OF WORK.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2A
PS&E	2025	30S034-M3-002	2A

GREENE COUNTY S.R. 34



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES
AND
UTILITY OWNERS

GENERAL NOTES

GUARDRAIL

- (1) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

FINAL PAVEMENT MARKING

- (1) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (1) THE PAVEMENT MARKING ON THE EDGELINE FOR LANE SHIFTS WILL BE INSTALLED AND MAINTAINED TO THE SAME STANDARDS AS FOR PERMANENT MARKINGS ON THE MAIN ROADWAY. THESE MARKINGS SHALL BE IN PLACE PRIOR TO ALLOWING TRAFFIC ONTO THE PAVEMENT. THESE PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.08 REMOVABLE PAVEMENT MARKING (6" LINE) (L.F.) OR 712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) (L.F.). SEE PLANS FOR DETAILS.
- (2) BEFORE OPENING THE LANE SHIFTS TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.08 REMOVABLE PAVEMENT MARKING (6" LINE) (L.F.) OR 712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) PER L.F. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LUMP SUM.

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.
- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- (3) THE CONTRACTOR SHALL ATTACH A DEVICE TO THE SCREED OF THE PAVER SUCH THAT MATERIAL IS CONFINED AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A CONSOLIDATED WEDGE-SHAPE PAVEMENT EDGE OF APPROXIMATELY 25 TO 30 DEGREES AS IT LEAVES THE PAVER (MEASURED FROM A LINE PARALLEL TO THE PAVEMENT SURFACE.) THE DEVICE SHALL MEET THE REQUIREMENTS THAT ARE CURRENTLY SET FORTH IN SPECIAL PROVISION 407SE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. **712-06**, SIGNS (CONSTRUCTION), S.F.

SPECIAL NOTES

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.

- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (10) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

INSPECTION, MAINTENANCE & REPAIR

- (12) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (13) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2B
PS&E	2025	30S034-M3-002	2B
GREENE COUNTY			S.R. 34

SEALD BY



03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
AND
SPECIAL NOTES

2/14/2025 11:41:20 AM
c:\pw_pl1\picke\dp\4294646\02B1_SpecialNotes.dgn

- (14)

EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (15)

DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (16)

UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (17)

INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (18)

THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (19)

SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (1)

SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2)

NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3)

INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5)

THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6)

STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE
- USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7)

HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.

(8)

WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.

(9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.
- #### SPECIES
- (10)

NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

(11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

(12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.
- #### PERMITS, PLANS & RECORDS
- (13)

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).

(14)

ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.

(15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

(16)

THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE

CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

(17)

ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

SUPPORT ACTIVITIES

(18)

MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ENVIRONMENTAL

(19)

EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

(1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

(2)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.

(3)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.

(4)

ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

(5)

SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

(1)

IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

(2)


AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.

(3)

UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2B1
PS&E	2025	30S034-M3-002	2B1
GREENE COUNTY			S.R. 34

SEALED BY



03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
AND
SPECIAL NOTES

2/14/2025 11:42:57 AM
c:\pw_pl1\picke\dp\04294646\02B2_SpecialNotes.dgn

- (4)

PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5)

CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6)

EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7)

TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9)

OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (10)

THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

INSPECTION, MAINTENANCE & REPAIR

- (12)

THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (13)

TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC "LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES" COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT "FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL" CLASS

- AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (14)

EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (15)

DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (16)

UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (17)

INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (18)

THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (19)

SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

EROSION PREVENTION

- (20)

CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (21)

THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (22)

NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.
- (23)

TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 CALENDAR DAYS WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 CALENDAR DAYS. PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 CALENDAR DAYS AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.
- (24)

STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (25)

PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.

- (26)

TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (27)

DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

PERMITS, PLANS & RECORDS

- (28)

THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)

CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)

WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2B2
PS&E	2025	30S034-M3-002	2B2
GREENE COUNTY			S.R. 34

SEALED BY

ALYSON FOSTER BOYD

REGISTERED ENGINEER

AGRICULTURE

COMMERCIAL

NO. 119,734

STATE OF TENNESSEE

03-25-2025

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

AND

SPECIAL NOTES

2/14/2025 11:44:19 AM
c:\pw_pl1\pickelp\04294646\02B3_SpecialNotes.dgn

- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SUPPORT ACTIVITIES

- (41) IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (42) MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (43) IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

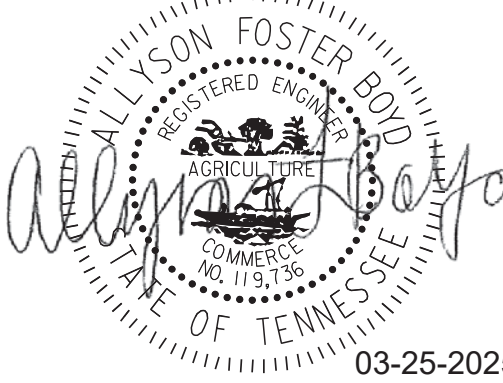
SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND

- MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.
- STREAMS, WETLANDS & BUFFER ZONES**
- (54) ANY WORK WITHIN THE STREAM CHANNEL AREA (E.G., PIER FOOTING, RIP-RAP PLACEMENT, CULVERT/BRIDGE CONSTRUCTION, ETC.) SHALL BE SEPARATED FROM FLOWING WATER OR EXPECTED FLOW PATH AND PERFORMED DURING LOW FLOW CONDITIONS. ALL ITEMS USED WITHIN THE STREAM CHANNEL AREA FOR DIVERSION OF FLOW (OR EXPECTED FLOW), UNLESS SPECIFIED IN THE PLANS, SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE COST OF OTHER ITEMS. THIS NOTE EXCLUDES ANY ITEMS SPECIFIED IN THE PLANS FOR THE TEMPORARY DIVERSION CHANNELS (EC-STR-31) AND TEMPORARY DIVERSION CULVERTS (EC STR-32) FOR SINGLE BARREL CULVERT CONSTRUCTION.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2B3
PS&E	2025	30S034-M3-002	2B3
GREENE COUNTY			S.R. 34

SEALED BY



03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
AND
SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	2C
PS&E	2025	30S034-M3-002	2C

GREENE COUNTY

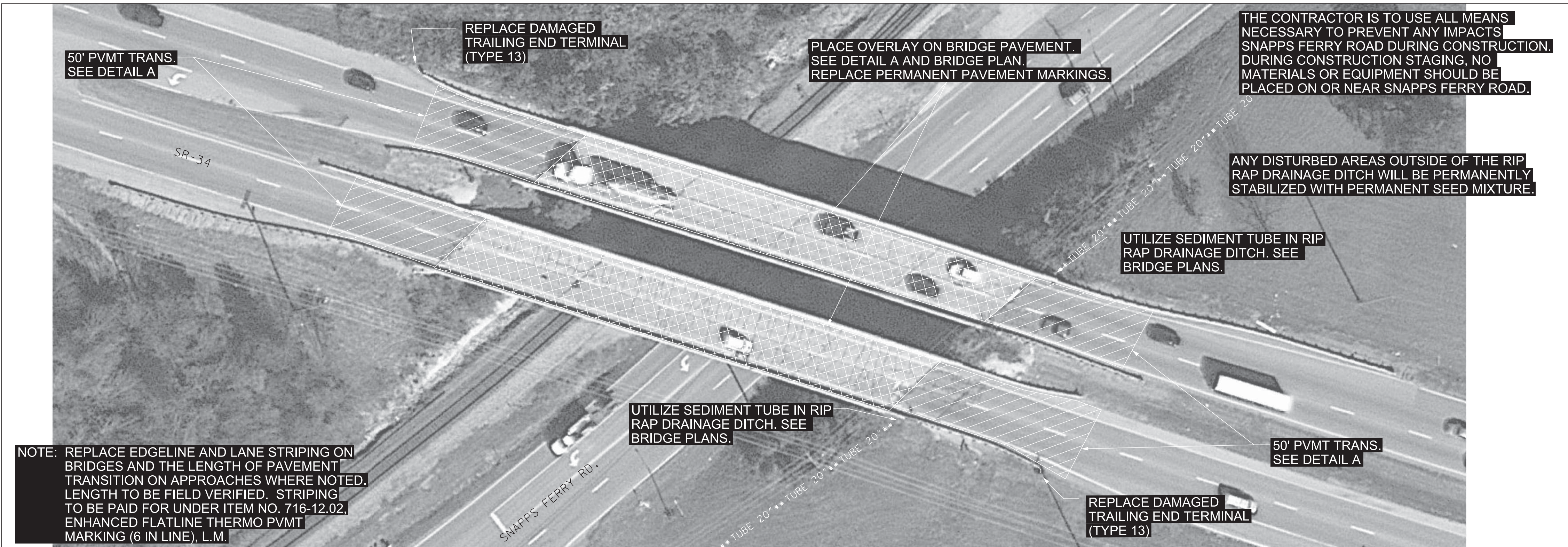
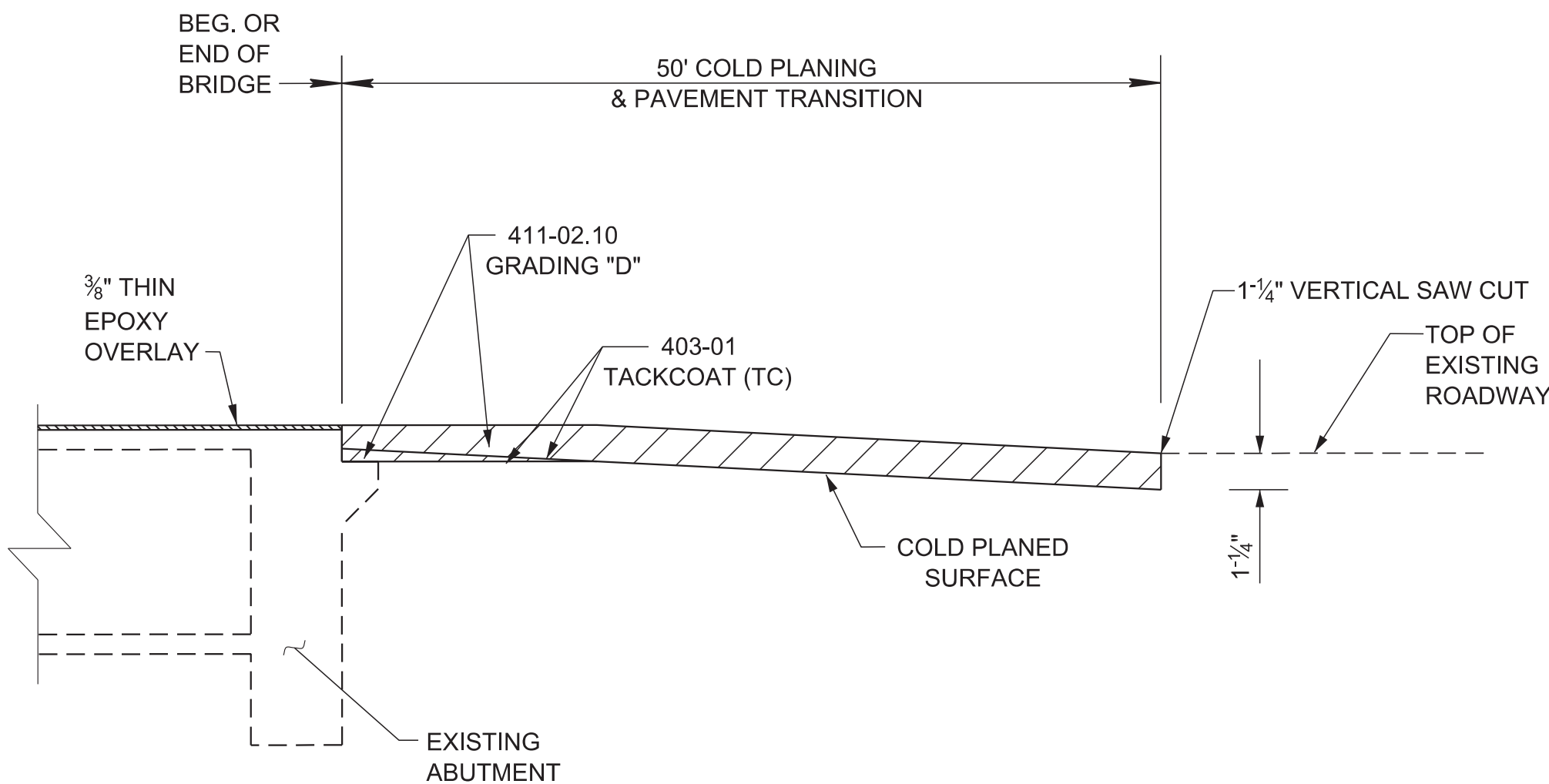
S.R. 34

DETAIL A

PAVEMENT TRANSITION DETAIL

N.T.S.

NOTE: SEE SECTIONS 617 AND 906.04 OF THE TENNESSEE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.



SEALED BY

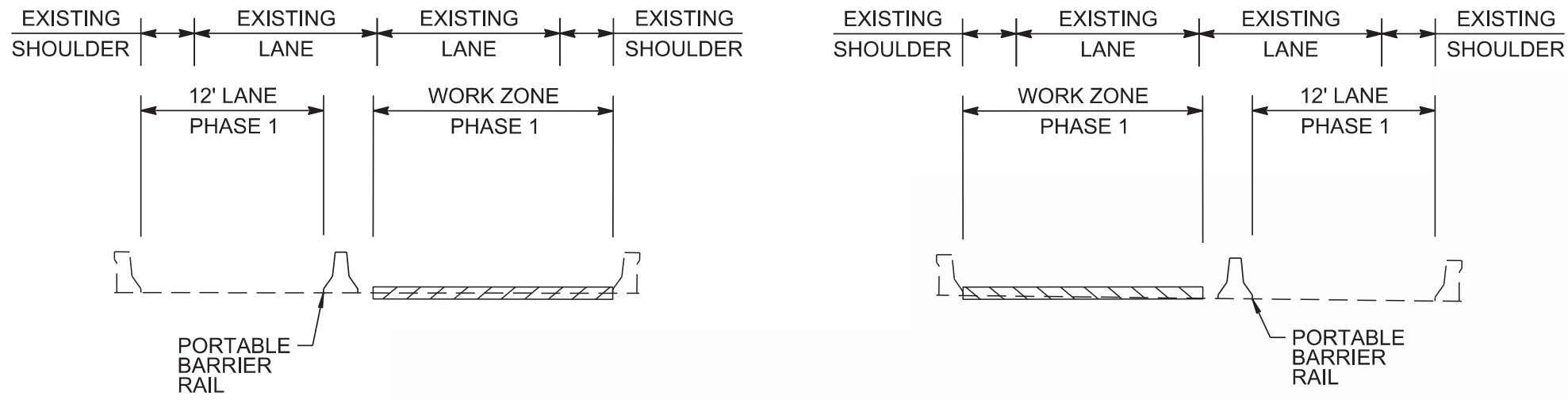


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GUARDRAIL PLAN
AND PAVEMENT
TRANSITION
DETAILS

S.R. 34 OVER SNAPPS FERRY RD.

2/14/2025 8:46:17 AM
c:\pw_pl1\pickel\p103_MOT_Phase 1-1.dgn

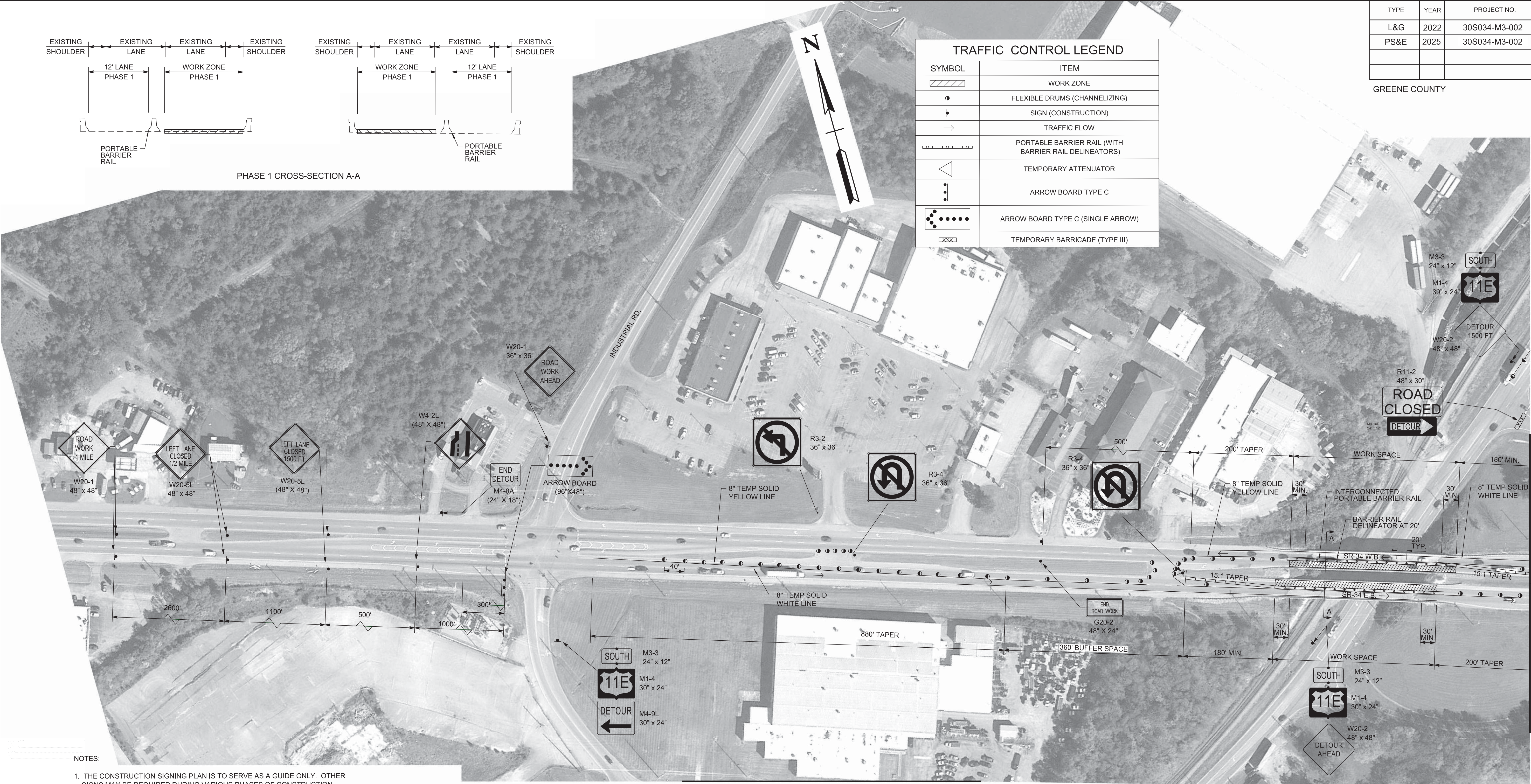


PHASE 1 CROSS-SECTION A-A

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)
	TEMPORARY BARRICADE (TYPE III)

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3
PS&E	2025	30S034-M3-002	3

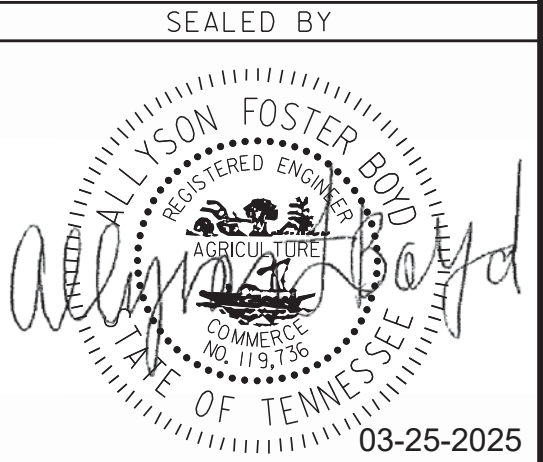
GREENE COUNTY S.R. 34



NOTES:

1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
6. SEE SHEET 2A FOR QUANTITIES.
7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

S.R. 34 OVER SNAPPS FERRY RD.
PHASE 1 TRAFFIC CONTROL PLAN

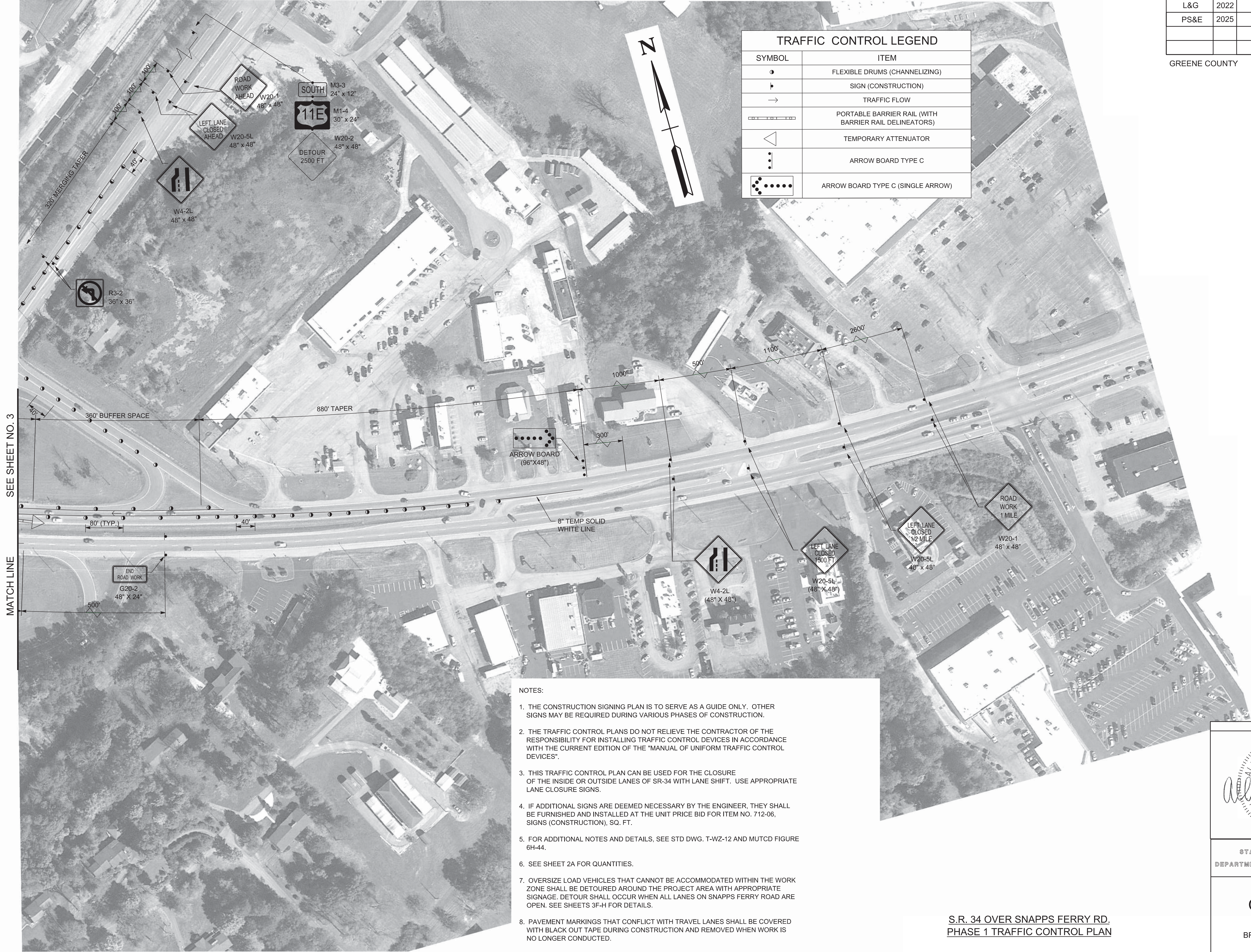


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN

BR. NO. 30-SR34-17.25
(LEFT & RIGHT)
SCALE: 1"=200'

2/14/2025 9:15:03 AM
c:\pw_pl1\pickel\p1\4294646103A_MOT_Phase 1-2.dgn



- NOTES:
1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
 2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
 3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
 8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

S.R. 34 OVER SNAPPS FERRY RD.
PHASE 1 TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3A
PS&E	2025	30S034-M3-002	3A
GREENE COUNTY			S.R. 34

SEALED BY

03-25-2025

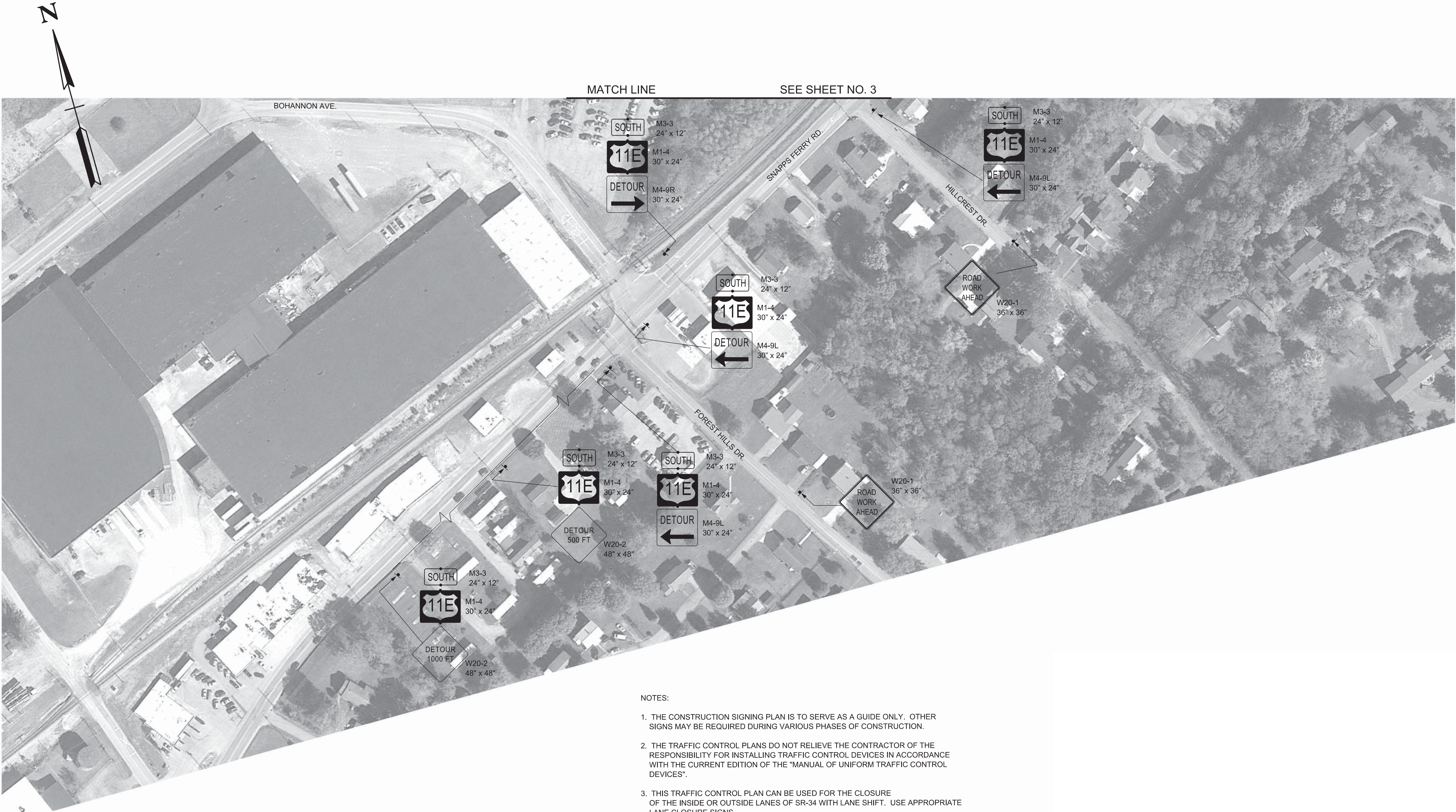
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 30-SR34-17.25
(LEFT & RIGHT)
SCALE: 1"=200'

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3B
PS&E	2025	30S034-M3-002	3B

GREENE COUNTY

S.R. 34




NOTES:

1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
6. SEE SHEET 2A FOR QUANTITIES.
7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

S.R. 34 OVER SNAPPS FERRY RD.
PHASE 1 TRAFFIC CONTROL PLAN

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)

SEALED BY



03-25-2025

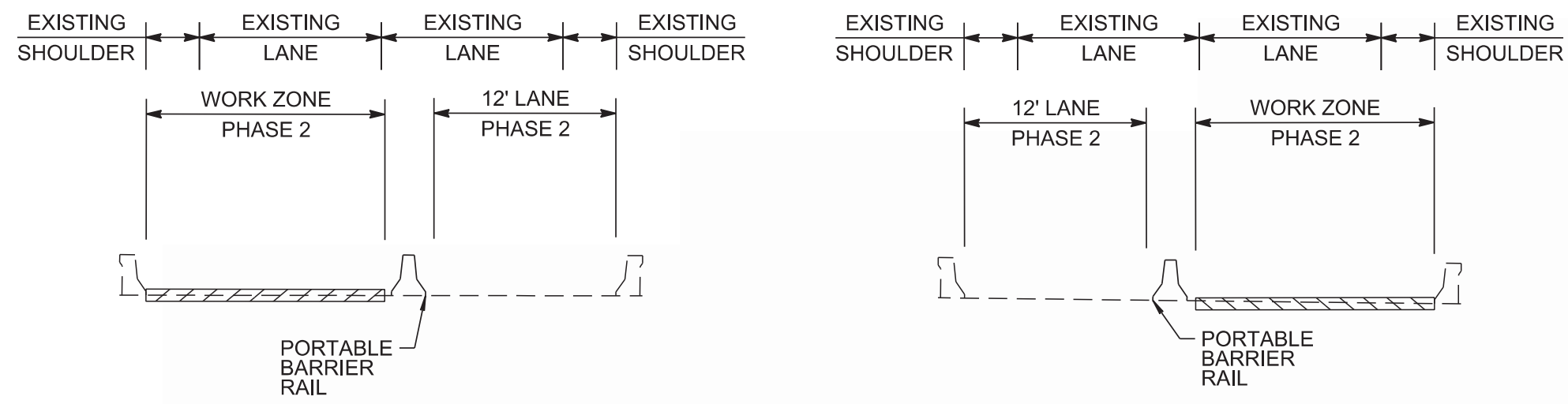
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 30-SR34-17.25
(LEFT & RIGHT)
SCALE: 1"=200'

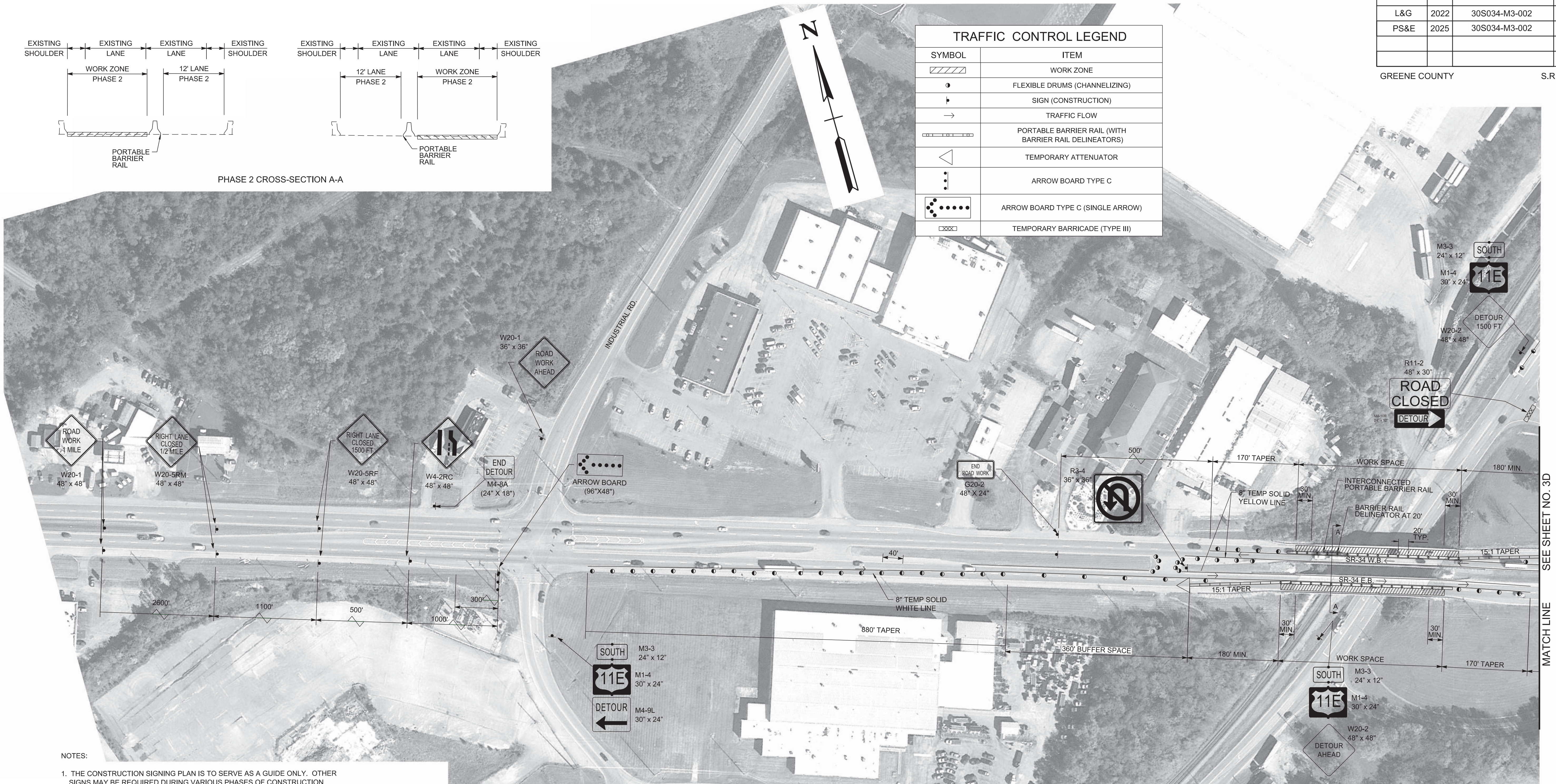
2/14/2025 9:37:25 AM
c:\pw_pl1\picke\dp\04294646103C_MOT_Phase 2-1.dgn

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3C
PS&E	2025	30S034-M3-002	3C

GREENE COUNTY S.R. 34



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)
	TEMPORARY BARRICADE (TYPE III)



NOTES:

1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
6. SEE SHEET 2A FOR QUANTITIES.
7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

S.R. 34 OVER SNAPPS FERRY RD.
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

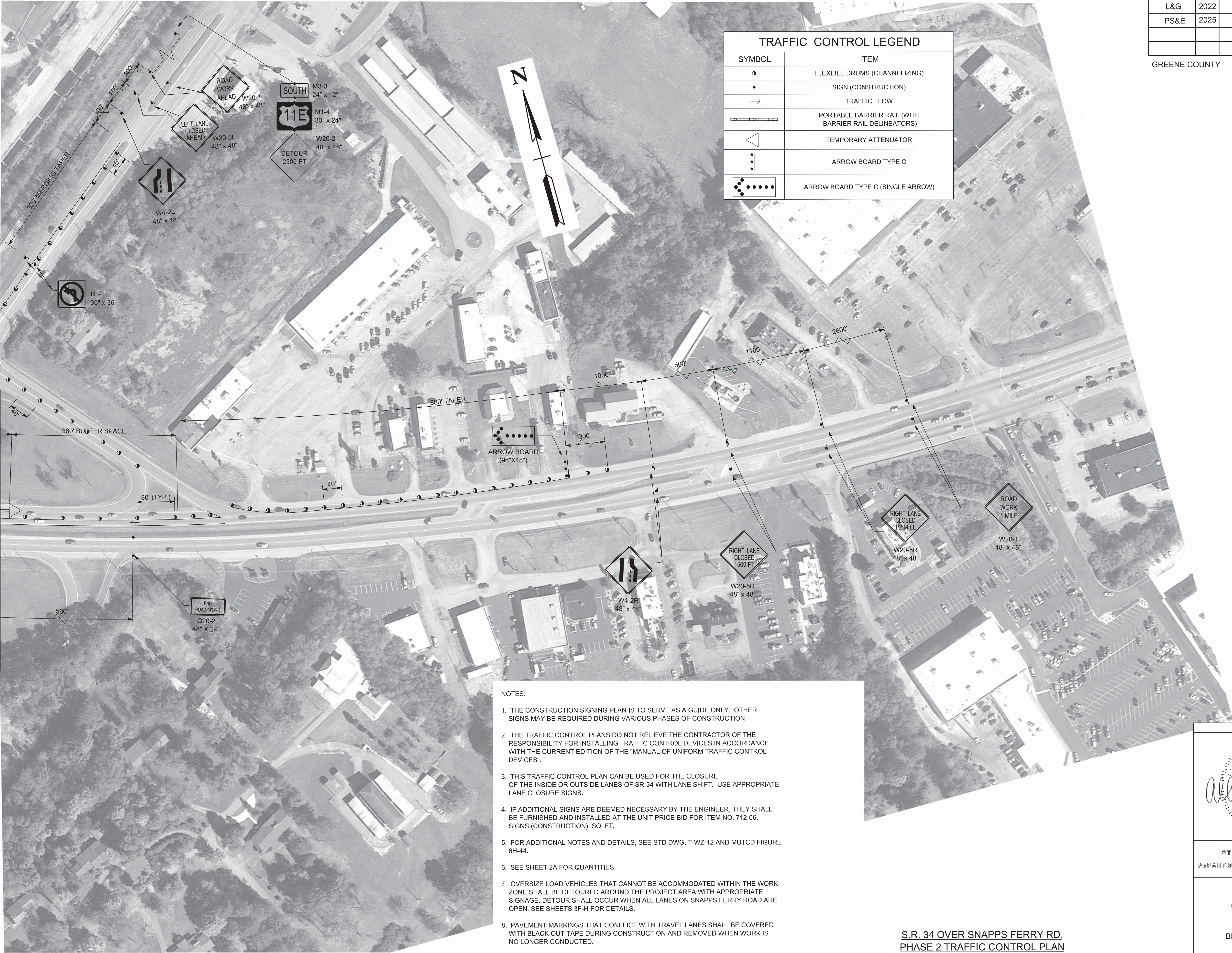
TRAFFIC CONTROL PLAN

BR. NO. 30-SR34-17.25
(LEFT & RIGHT)
SCALE: 1"=200'

2/14/2025 10:13:49 AM
c:\pw_plt\pickel\p\d4294646103D_MOT_Phase 2-2.dgn

SEE SHEET NO. 3B

MATCH LINE



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)

NOTES:

1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
6. SEE SHEET 2A FOR QUANTITIES.
7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

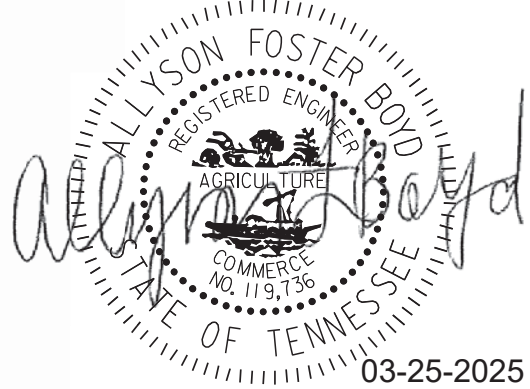
S.R. 34 OVER SNAPPS FERRY RD.
PHASE 2 TRAFFIC CONTROL PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3D
PS&E	2025	30S034-M3-002	3D

GREENE COUNTY

S.R. 34

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

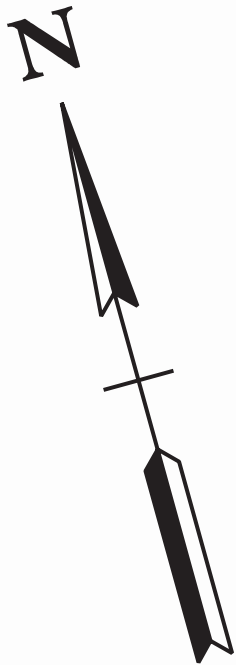
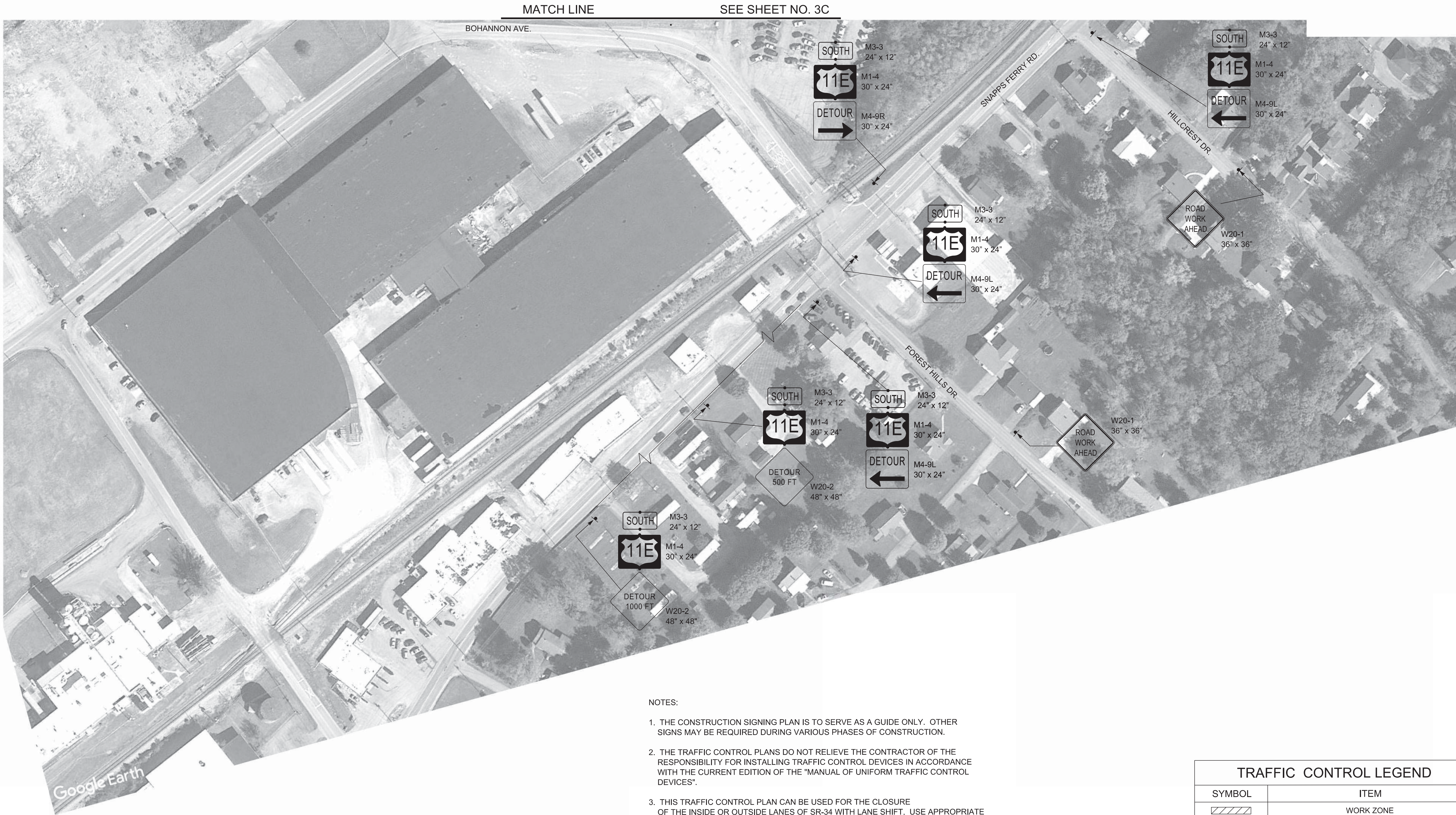
TRAFFIC CONTROL PLAN

BR. NO. 30-SR34-17.25
(LEFT & RIGHT)
SCALE: 1"=200'

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3E
PS&E	2025	30S034-M3-002	3E

GREENE COUNTY

S.R. 34



NOTES:

1. THE CONSTRUCTION SIGNING PLAN IS TO SERVE AS A GUIDE ONLY. OTHER SIGNS MAY BE REQUIRED DURING VARIOUS PHASES OF CONSTRUCTION.
2. THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
3. THIS TRAFFIC CONTROL PLAN CAN BE USED FOR THE CLOSURE OF THE INSIDE OR OUTSIDE LANES OF SR-34 WITH LANE SHIFT. USE APPROPRIATE LANE CLOSURE SIGNS.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE STD DWG. T-WZ-12 AND MUTCD FIGURE 6H-44.
6. SEE SHEET 2A FOR QUANTITIES.
7. OVERSIZE LOAD VEHICLES THAT CANNOT BE ACCOMMODATED WITHIN THE WORK ZONE SHALL BE DETOURED AROUND THE PROJECT AREA WITH APPROPRIATE SIGNAGE. DETOUR SHALL OCCUR WHEN ALL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE SHEETS 3F-H FOR DETAILS.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL (WITH BARRIER RAIL DELINEATORS)
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)

S.R. 34 OVER SNAPPS FERRY RD.
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN

BR. NO. 30-SR34-17.25
(LEFT & RIGHT)

SCALE: 1"=200'

2/14/2025 10:34:04 AM
c:\pw_pl1\pickeidp\d4294646103F_MOT_Phase 2A-1.dgn

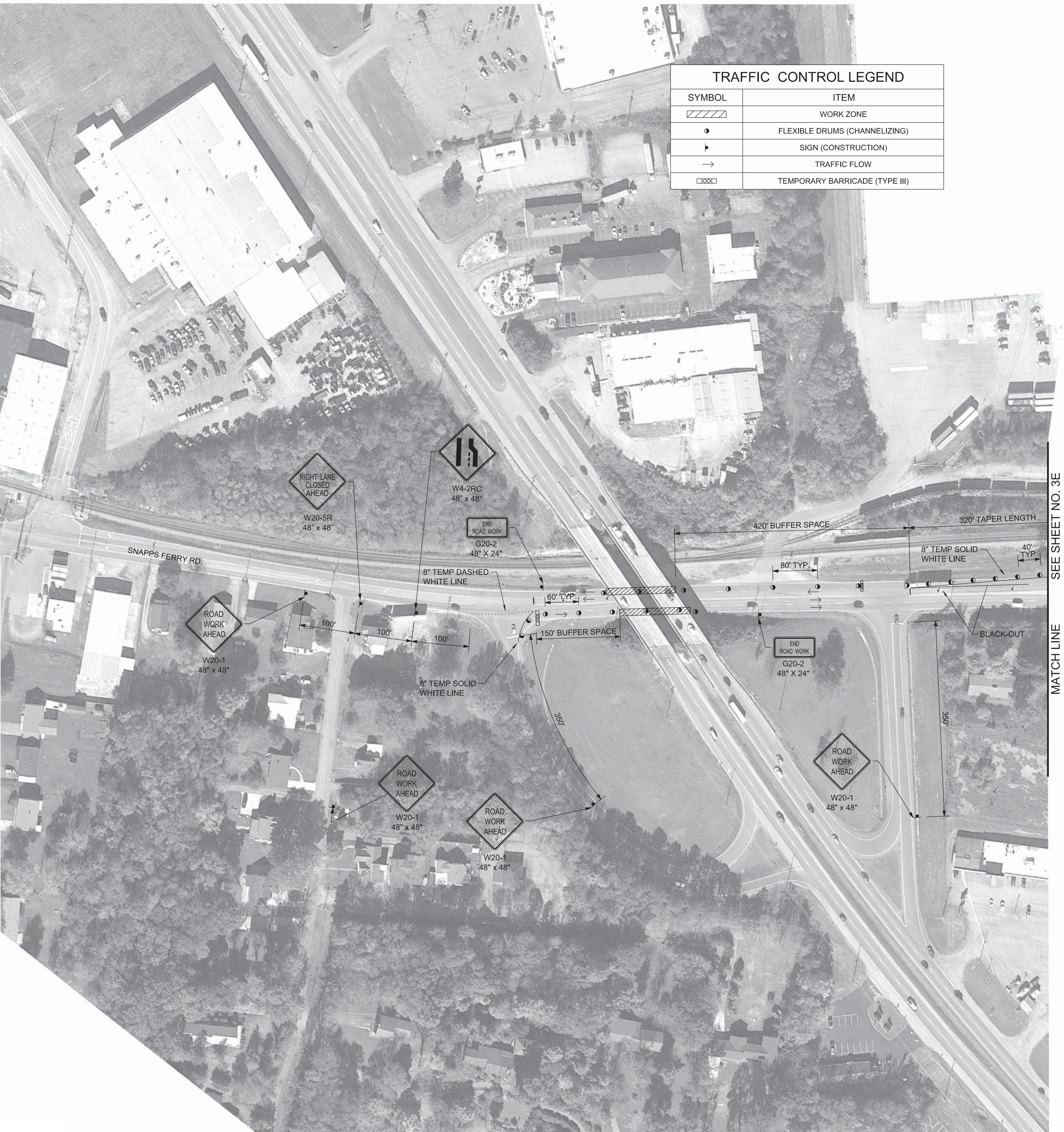
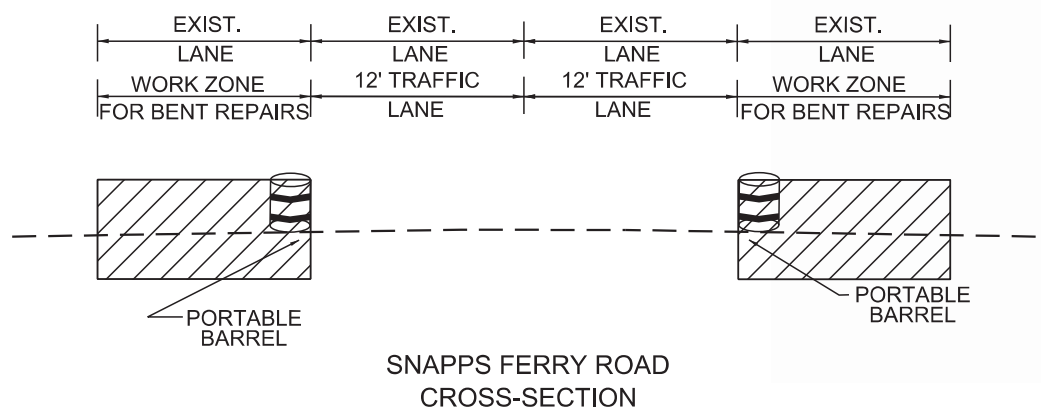
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3F
PS&E	2025	30S034-M3-002	3F

GREENE COUNTY

S.R. 34

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	TEMPORARY BARRICADE (TYPE III)

- NOTES:
1. LANE CLOSURES TO BE PROVIDED DURING BRIDGE BENT REPAIRS THAT CANNOT BE CONDUCTED FROM TOP OF BRIDGE DECK. WHEN WORK CAN BE PERFORMED FROM TOP OF BRIDGE DECK, ADVANCE WARNING SIGNS OF LANE CLOSURE MUST BE FULLY COVERED AND ALL TRAVEL LANES MUST BE OPEN TO THE PUBLIC.
 2. ALL TRAVEL LANES ON SR-34 MUST BE OPEN TO PUBLIC TO ALLOW FOR OVERSIZE VEHICLE TRAVEL ON SR-34. WIDE LOAD DETOUR CANNOT OCCUR DURING SNAPPS FERRY LANE CLOSURES.
 3. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 4. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 5. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 6. FOR ADDITIONAL NOTES AND DETAILS, TDOT STD. DWG. T-WZ-40.
 7. SEE SHEET 2A FOR QUANTITIES.
 8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.



SEALED BY

03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN

SNAPPS FERRY ROAD

SCALE: 1"=100'

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3G
PS&E	2025	30S034-M3-002	3G

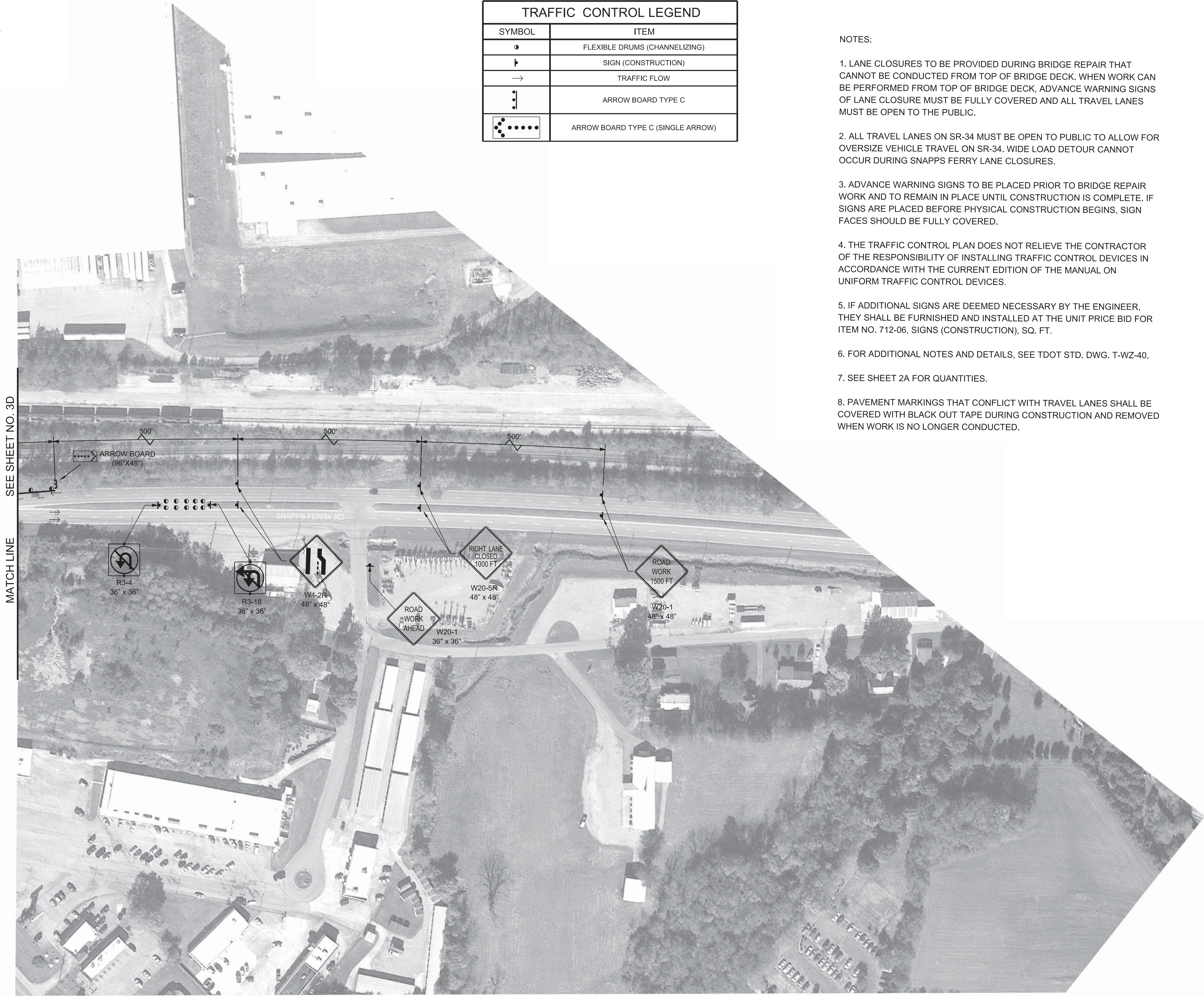
GREENE COUNTY

S.R. 34

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	ARROW BOARD TYPE C
	ARROW BOARD TYPE C (SINGLE ARROW)

NOTES:

1. LANE CLOSURES TO BE PROVIDED DURING BRIDGE REPAIR THAT CANNOT BE CONDUCTED FROM TOP OF BRIDGE DECK. WHEN WORK CAN BE PERFORMED FROM TOP OF BRIDGE DECK, ADVANCE WARNING SIGNS OF LANE CLOSURE MUST BE FULLY COVERED AND ALL TRAVEL LANES MUST BE OPEN TO THE PUBLIC.
2. ALL TRAVEL LANES ON SR-34 MUST BE OPEN TO PUBLIC TO ALLOW FOR OVERSIZE VEHICLE TRAVEL ON SR-34. WIDE LOAD DETOUR CANNOT OCCUR DURING SNAPPS FERRY LANE CLOSURES.
3. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
4. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
5. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
6. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-40.
7. SEE SHEET 2A FOR QUANTITIES.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.



SEALED BY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

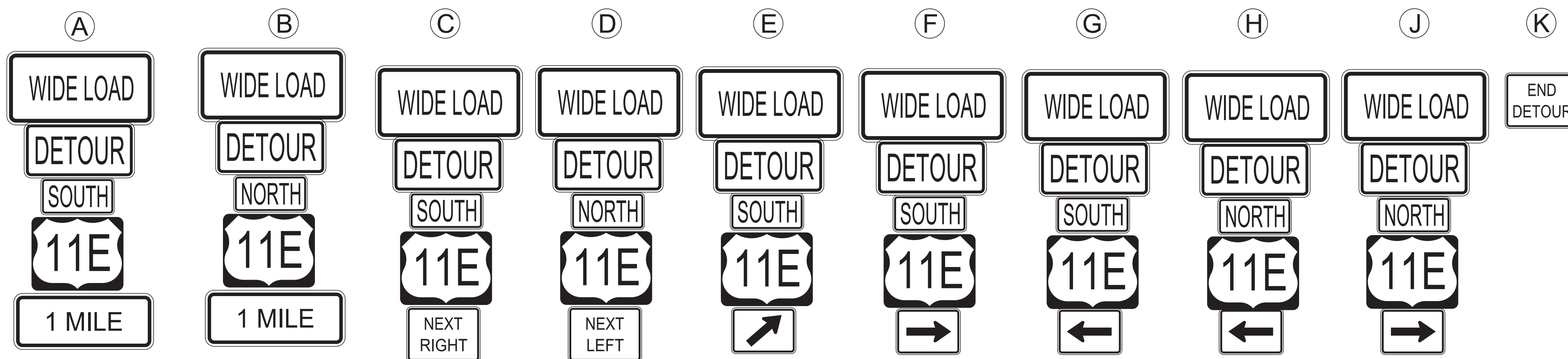
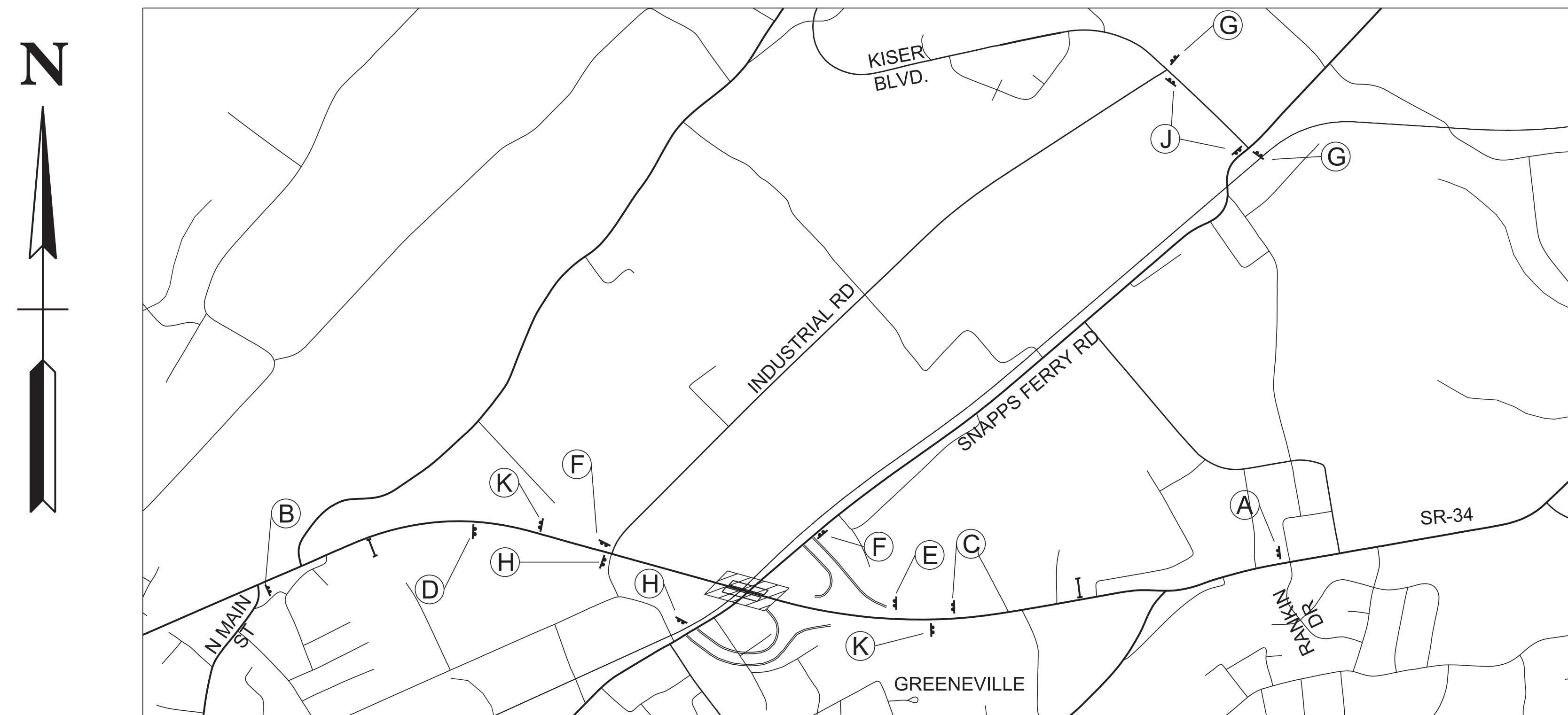
TRAFFIC CONTROL PLAN

SNAPPS FERRY ROAD

SCALE: 1"=100'




TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2022	30S034-M3-002	3H
PS&E	2025	30S034-M3-002	3H

GREENE COUNTY S.R. 34



DETOUR NETWORK NOTES

1. CONTRACTOR TO INSTALL DETOUR SIGNS ALONG CITY STREETS PENDING COORDINATION AND APPROVAL FROM THE TOWN OF GREENEVILLE CITY MANAGER, TODD SMITH (TSMITH@GREENEVILLETN.GOV, 423.639.7105 (WORK), 423-787-6184 (MOBILE)).
2. WIDE LOAD DETOUR TO OCCUR DURING LANE CLOSURES ON SR-34 AND WHEN ALL TRAVEL LANES ON SNAPPS FERRY ROAD ARE OPEN. SEE BRIDGE SHEETS BR-01 & 03 AND TRAFFIC CONTROL PLANS FOR PHASING AND LANE RESTRICTIONS.
3. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO CONSTRUCTION AND TO REMAIN IN PLACE UNTIL THE COMPLETION OF THIS PROJECT. ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
4. MESSAGE BOARDS IN ADVANCE OF THE DETOUR AREA MAY BE OPERATIONAL PRIOR TO LANE CLOSURE TO INFORM PUBLIC OF DATES OF LANE CLOSURES.
5. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
6. THE LOCATION OF ALL TRAFFIC CONTROL DEVICES ARE TO BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING. CONTRACTOR TO INSTALL WIDE LOAD DETOUR SIGNS AND MESSAGE BOARDS IN TENNESSEE PENDING COORDINATION AND APPROVAL FROM TDOT.
7. IF THE CONTRACTOR MOVES OFF THE PROJECT, THEY SHALL COVER OR REMOVE ALL UN-NEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
8. SPECIAL DETOUR SIGNING SHALL BE BLACK COPY ON ORANGE BACKGROUND EXCEPT THAT CARDINAL DIRECTIONS AND INTERSTATE SHIELDS SHALL BE STANDARD INTERSTATE SIGNS.

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION) (2-POST)
	CHANGEABLE MESSAGE SIGN

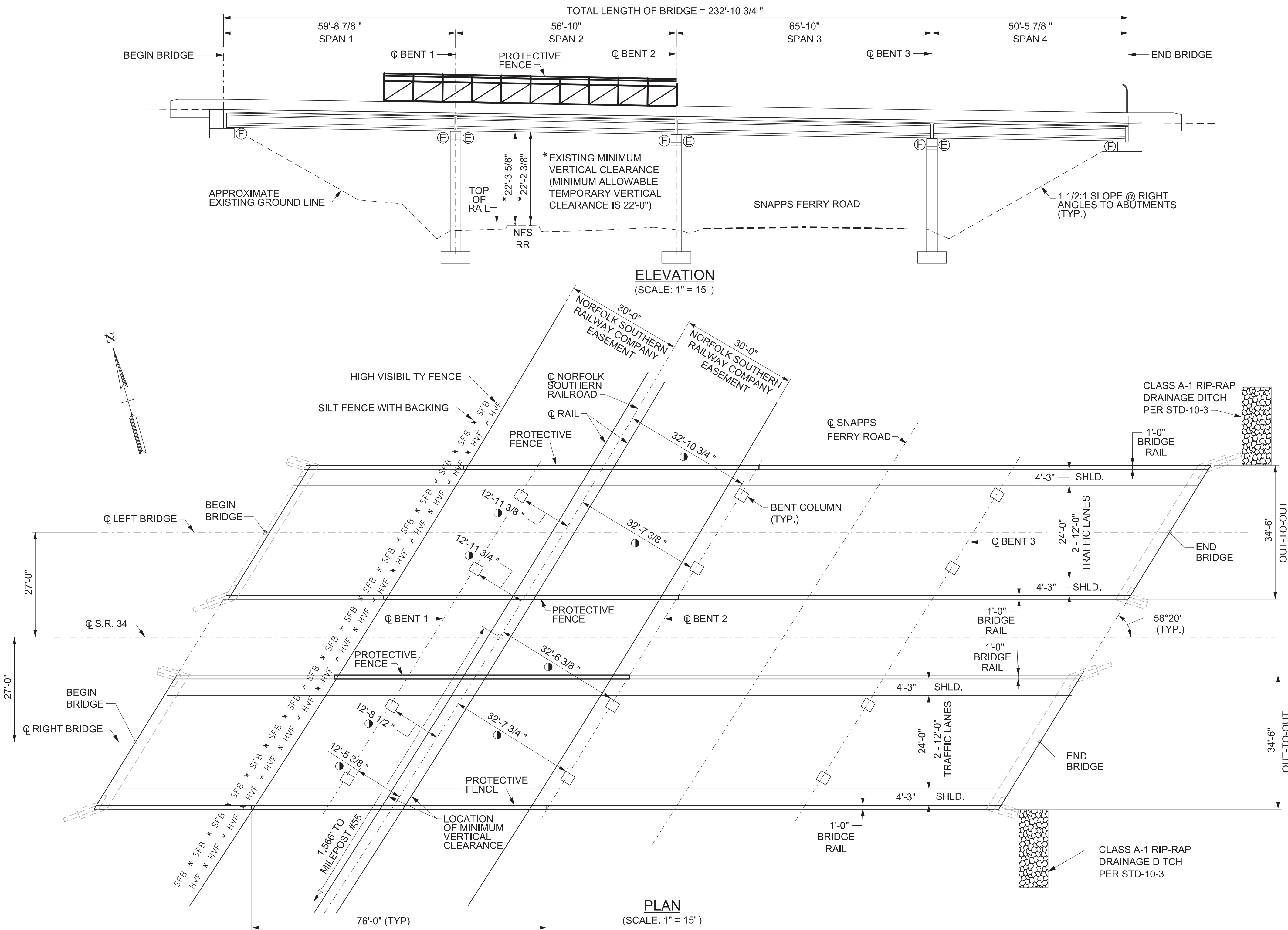
SEAL BY

ALLISON FOSTER BOYD
REGISTERED ENGINEER
STATE OF TENNESSEE
COMMERCIAL
NO. 115,713
DATE OF TENNESSEE

03-25-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL
PLAN
WIDE LOAD DETOUR
NOT TO SCALE



SCOPE OF WORK

- ① LOCATION OF MINIMUM HORIZONTAL CLEARANCE (MINIMUM ALLOWABLE TEMPORARY HORIZONTAL CLEARANCE IS 14'-0")

NOTE:
EXISTING SUBSTANDARD CLEARANCES SHALL NOT BE FURTHER REDUCED FOR THE TEMPORARY CONSTRUCTION CONDITION WITHOUT WRITTEN PERMISSION FROM NS.

1. PROVIDE REQUIRED TRAFFIC CONTROL AND PHASE CONSTRUCTION MAINTAINING ONE 12 FOOT LANE OF TRAFFIC IN EACH DIRECTION ALONG SR 34 AT ALL TIMES.
2. SHOTBLAST BRIDGE DECKS TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPPING, OR ANY OTHER DETRIMENTAL MATERIAL.
3. RECONSTRUCT THE CONCRETE BRIDGE DECKS IN AREAS OF FULL AND PARTIAL DEPTH DECK REPAIR UTILIZING 18 HOUR HIGH EARLY STRENGTH CONCRETE.
4. SEAL REMAINING CRACKS ON CONCRETE BRIDGE DECKS WITH HMWM. COST TO BE INCLUDED IN TYPE 1 THIN EPOXY OVERLAY.
5. REMOVE THE EXISTING JOINT SEAL SYSTEM AT BENT 1, LEFT AND RIGHT LANES AND REPLACE WITH A COMPRESSION JOINT SEAL SYSTEM.
6. PLACE TYPE 1 THIN EPOXY OVERLAY SYSTEM OVER ENTIRE DECKS.
7. PLACE DELINEATORS ON TOP OF PARAPETS WITHIN LIMITS OF BRIDGE (COST TO BE INCLUDED IN ITEMS BID ON).
8. INSTALL PROTECTIVE FENCING ON PARAPETS OVER RAILROAD.
9. CONSTRUCT RIP-RAP DRAINAGE DITCH IN ACCORDANCE WITH STD-10-3 AT END OF BRIDGES ON RIGHT SIDE, RIGHT LANE AND LEFT SIDE, LEFT LANE.

10. CLEAN AND MAINTAIN END OF BRIDGE DRAINS (COST TO BE INCLUDED IN ITEMS BID ON).
11. REPAIR DETERIORATED AREAS OF CONCRETE ON PRESTRESSED PRECAST CONCRETE GIRDERS, AND SUBSTRUCTURE UNITS.
12. EPOXY INJECT CRACKS ON PRESTRESSED PRECAST CONCRETE GIRDERS, SUBSTRUCTURE UNITS, AND BRIDGE RAILS.
13. REMOVE BAGS AT LEFT LANE, BEGIN BRIDGE AND RIGHT LANE, BEGIN BRIDGE AND ENCASE EXPOSED PILINGS AT ABUTMENTS.
14. HIGH PRESSURE WATER WASH AND TEXTURE COAT ALL EXPOSED CONCRETE SURFACES OF BENTS, WINGWALLS, PARAPETS, DECK OVERHANGS, AND OUTSIDE FACE AND BOTTOM FLANGE OF EXTERIOR GIRDERS. TOP AND INSIDES FACE OF PARAPET SHALL BE WHITE. ALL OTHER SURFACES SHALL BE MOUNTAIN GREY.
15. REPLACE DAMAGED GUARDRAIL ENDS AT LEFT LANE, BEGIN BRIDGE AND RIGHT LANE, END BRIDGE.
16. PLACE NEW STRIPING AND SIGNING AS APPROPRIATE.

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

ⓔ - DENOTES EXPANSION
ⓕ - DENOTES FIXED

LIST OF BRIDGE DRAWINGS

DRAWING	DRAWING NO.	LATEST REV. DATE
LAYOUT OF BRIDGES TO BE REPAIRED	BR-131-902	
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES	BR-131-903	
PHASE CONSTRUCTION	BR-131-904	
SUPERSTRUCTURE REPAIRS	BR-131-905	
ABUTMENT REPAIRS LEFT BRIDGE	BR-131-906	
ABUTMENT REPAIRS RIGHT BRIDGE	BR-131-907	
BENT REPAIRS BENT 1 - LEFT BRIDGE	BR-131-908	
BENT REPAIRS BENT 2 - LEFT BRIDGE	BR-131-909	
BENT REPAIRS BENT 3 - LEFT BRIDGE	BR-131-910	
BENT REPAIRS BENT 1 - RIGHT BRIDGE	BR-131-911	
BENT REPAIRS BENT 2 - RIGHT BRIDGE	BR-131-912	
BENT REPAIRS BENT 3 - RIGHT BRIDGE	BR-131-913	
REPAIR DETAILS	BR-131-914	
CONCRETE REPAIR DETAILS	BR-131-915	

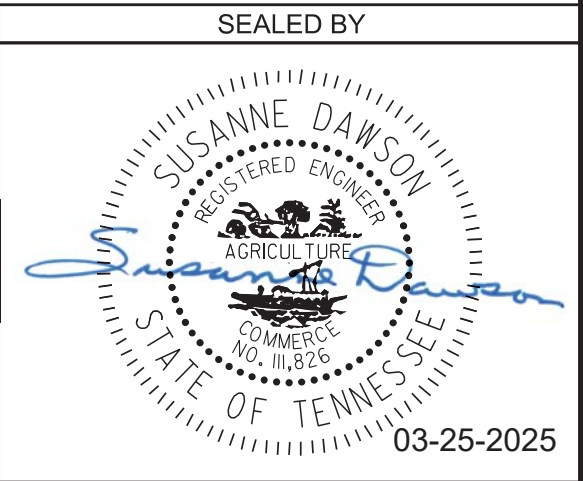
LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)
BR-16-74, BR-16-76 THRU BR-16-81,
K-27-108 THRU K-27-112 AND K-15-65

STANDARD BRIDGE DRAWINGS

DRAWING NO.	LATEST REV. DATE	DRAWING
STD-8-5	05/10/2021	STANDARD PROTECTIVE FENCE DETAILS
STD-10-3	01/10/2024	STANDARD FLUME DETAILS

TRAFFIC DATA	
S.R. 34	POSTED SPEED LIMIT = 45 M.P.H. ADT (2025) = 30,630
SNAPPS FERRY ROAD	POSTED SPEED LIMIT = 40 M.P.H. ADT (2025) = 6,359



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGES
TO BE REPAIRED
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025
BR-131-902

PIN NO.:	082699.01	DATE: AUGUST 2021
DESIGN BY:	SUSANNE DAWSON	DATE: AUGUST 2021
DRAWN BY:	DONNIE PICKEL	DATE: AUGUST 2021
SUPERVISED BY:	SUSANNE DAWSON	DATE: AUGUST 2021
CHECKED BY:	FRANK BALE	DATE: AUGUST 2021

GENERAL NOTES

SPECIFICATIONS & LOADING

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), AND THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.

DESIGN SPECIFICATIONS: 9TH EDITION (2020) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

STEEL, CONCRETE, REINFORCING, AND FORMING

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

HIGH EARLY STRENGTH CONCRETE: THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS "X". THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.

CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

MISCELLANEOUS GENERAL NOTES

SHOP DRAWINGS: REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02 OF THE STANDARD SPECIFICATIONS. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.

EXPANSION JOINTS: FOR ADDITIONAL GENERAL NOTES APPLICABLE TO STRIP SEAL EXPANSION JOINTS, SEE STANDARD DRAWING NOS. SBR-2-115 AND SBR-2-116, ALSO SEE SECTION 623 OF THE STANDARD SPECIFICATIONS.

SPECIAL NOTE FOR RAILROAD CROSSINGS: THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY THE NORFOLK SOUTHERN RAILROAD SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAFFIC.

DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. FOR FULL DEPTH SLAB REMOVAL, EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH SLAB REMOVAL AND ANY WORK OVER THE BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS; CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. EXPANSION JOINT REMOVAL SHALL FOLLOW THE SAME RESTRICTIONS AS FULL DEPTH SLAB REMOVAL. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.

QUICK-SET PATCHING MATERIAL: QUICK-SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTITIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13.009 POLY MOD CEMENT STRUCT PATCH VERT & OVER FOR ACCEPTABLE PATCHING MATERIALS.

FORMS AND FALSEWORK: CONCRETE FORM WORK, FALSEWORK AND TEMPORARY SUPPORTS SHALL BE REMOVED FROM THE JOB SITE AFTER WORK IS COMPLETED. COST OF FORMS, FALSEWORK AND TEMPORARY SUPPORTS SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.

PROTECTIVE FENCE: RAILROAD PROTECTIVE FENCE IS REQUIRED TO BE BUILT IN ACCORDANCE WITH STANDARD DRAWING STD-8-5. DIMENSION 'H' AS SHOWN ON STANDARD DRAWING STD-8-5 SHALL BE 10'-0".

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

ANY AREA THAT IS DISTURBED OUTSIDE THE LIMITS OF THE CONSTRUCTION DURING THE LIFE OF THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

ESTIMATED BRIDGE QUANTITIES						
ITEM NO.	DESCRIPTION	UNIT	30-SR034-17.25 L	30-SR034-17.25 R	TOTAL	
604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	1054	1056	2110	
604-10.05	CONCRETE	S.F.	214	288	502	
604-10.30	BRIDGE DECK REPAIR (FULL DEPTH OF SLAB)	S.Y.	5	5	10	
604-10.34	CONCRETE OVERLAYED BRIDGE DECK REPAIR (FULL DEPTH)	S.Y.	150	150	300	
604-10.44	EXPANSION JOINT REPAIRS	L.F.	41	41	82	
604-10.50	BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB)	S.Y.	5	5	10	
604-10.54	CONCRETE REPAIRS	S.F.	46	63	109	
604-10.55	CONCRETE (FOUNDATION REPAIRS)	C.Y.	4	1	5	
604-10.56	CONCRETE OVERLAYED BRIDGE DECK REPAIR (PARTIAL DEPTH)	S.Y.	20	20	40	
604-10.58	EPOXY INJECTION (INJECTION)	GAL.	2	3	5	
604-10.62	EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)	L.F.	46	57	103	
617-04.01	TYPE 1 THIN OVERLAY (EPOXY URETHANE)	S.Y.	802	802	1604	
707-07.01	CHAIN-LINK FENCE (BRIDGES)	S.F.	1115	1115	2230	

* DENOTES ITEM CAN BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

FOOTNOTES

- ① INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE APPLICATION OF ALL TEXTURE COATING FOR THE FULL LENGTH OF THE BRIDGE AS SHOWN IN THE TEXTURE COATING DETAIL ON THIS SHEET. ALSO INCLUDES SURFACE PREPARATION USING A HIGH PRESSURE WASH TO REMOVE ALL LOOSE COATINGS, FLAKING, AND OTHER FOREIGN SUBSTANCES TO THE FULL SATISFACTION OF THE ENGINEER.
- ② INCLUDES ALL LABOR AND MATERIALS NECESSARY TO PLACE HIGH EARLY STRENGTH CONCRETE FOR REPAIR OF INDICATED AREAS.
- ③ INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY TO PLACE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL FOR REPAIR OF INDICATED AREAS.
- ④ INCLUDES ALL LABOR AND MATERIALS NECESSARY TO REPLACE EXPANSION JOINT SEALS AT BENT 1

RAILROAD NOTES

THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY EACH INDIVIDUAL RAILROAD BEING AFFECTED BY THIS PROJECT AT THE PRECONSTRUCTION MEETING SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAFFIC IN ANY MANNER.

FOR A GENERAL LIST OF ANTICIPATED CONSTRUCTION SUBMISSIONS, PLEASE REVIEW SECTION 6.A.2 OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAYS INTERESTS. A LIST OF REQUIRED SUBMISSIONS WILL BE PROVIDED AT THE PRECONSTRUCTION MEETING.

THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING SITE ACCESS, INCLUDING ANY NECESSARY TEMPORARY CROSSING, TO PERFORM CONSTRUCTION ACTIVITIES WITHIN NS ROW TO NS FOR REVIEW AND APPROVAL.

THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURE NECESSARY TO PREVENT DEBRIS FROM FALLING ONTO THE RAILROAD'S RIGHT-OF-WAY. THE METHOD PROPOSED TO ACCOMPLISH THIS MUST BE SUBMITTED TO NORFOLK SOUTHERN FOR REVIEW AND APPROVAL PRIOR TO USE AND MUST NOT INFRINGE ON THE VERTICAL AND/OR HORIZONTAL CLEARANCES IN THESE PLANS. THE TEMPORARY MINIMUM VERTICAL AND HORIZONTAL CONSTRUCTION CLEARANCES THAT THE CONTRACTOR MUST MEET AT ALL TIMES ARE LISTED IN SECTION 5.A OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAYS INTERESTS, WHICH CRITERIA ARE INCLUDED IN THE SPECIAL PROVISION 105C. NORFOLK SOUTHERN CORPORATION WILL NOT ALLOW TRACK OR TRACKS TO BE OBSTRUCTED WITH AT GRADE CRANE MAT PROTECTION. COMPLETE AND FULL ENCLOSURE OF STRUCTURE DEMOLITION WITHIN ALLOWABLE CLEARANCES WILL BE REQUIRED. THE COST OF REMOVING AND DISPOSING OF DEBRIS, AND THE COMPLETE AND FULL PROTECTION OF THE CONTRACTOR'S WORK AREA FOR DEMOLITION SHALL BE INCLUDED IN UNIT ITEMS BID ON.

CONTRACTOR PROTECTIVE SERVICES SHALL BE ONSITE ANYTIME CONSTRUCTION ACTIVITIES ARE TAKING PLACE ON OR ADJACENT TO THE RAILROAD PROPERTY AND/OR HAVE THE POTENTIAL TO FOUL THE RAILROAD'S TRACK OR OPERATIONS. SEE SECTION 8 OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAY INTERESTS - DIRECT HIRE FOR ADDITIONAL CLARIFICATIONS AND REQUIREMENTS REGARDING CONTRACTOR PROTECTIVE SERVICES.

PER SECTION H.1.5.J OF THE NS PUBLIC PROJECTS MANUAL, NO DRAINAGE IS PERMITTED TO DRAIN ONTO RAILROAD PROPERTY. THIS INCLUDES ANY WASH WATER FROM THE CLEANING OF EXPOSED CONCRETE SURFACES. HOWEVER, EXISTING DRAINS MAY REMAIN WITHOUT MODIFICATIONS. THE CONTRACTOR SHALL MAKE PROVISIONS TO COLLECT AND CONTAIN ALL WASH WATER TO ENSURE IT DOES NOT FALL ON TO RAILROAD PROPERTY. THE CONTRACTOR SHALL NOT DISCHARGE WASH WATER ON TO RAILROAD PROPERTY.

ALL WORK ON, OVER, UNDER, OR ADJACENT TO NORFOLK SOUTHERN (NS) RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH THE NORFOLK SOUTHERN "SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS" (NS SPECIAL PROVISIONS).

THE CONTRACTOR SHALL SUBMIT UNDER SEPARATE COVER A DEMOLITION DEBRIS SHIELD INSTALLATION PLAN.

CONTRACTOR WILL NOT BE PERMITTED TO STORE ANY EQUIPMENT ON NS PROPERTY WITHOUT PERMISSION FROM THE NS RAILROAD ENGINEER IN ACCORDANCE WITH SECTION E.5.K.1 OF THE NS PUBLIC PROJECTS MANUAL.

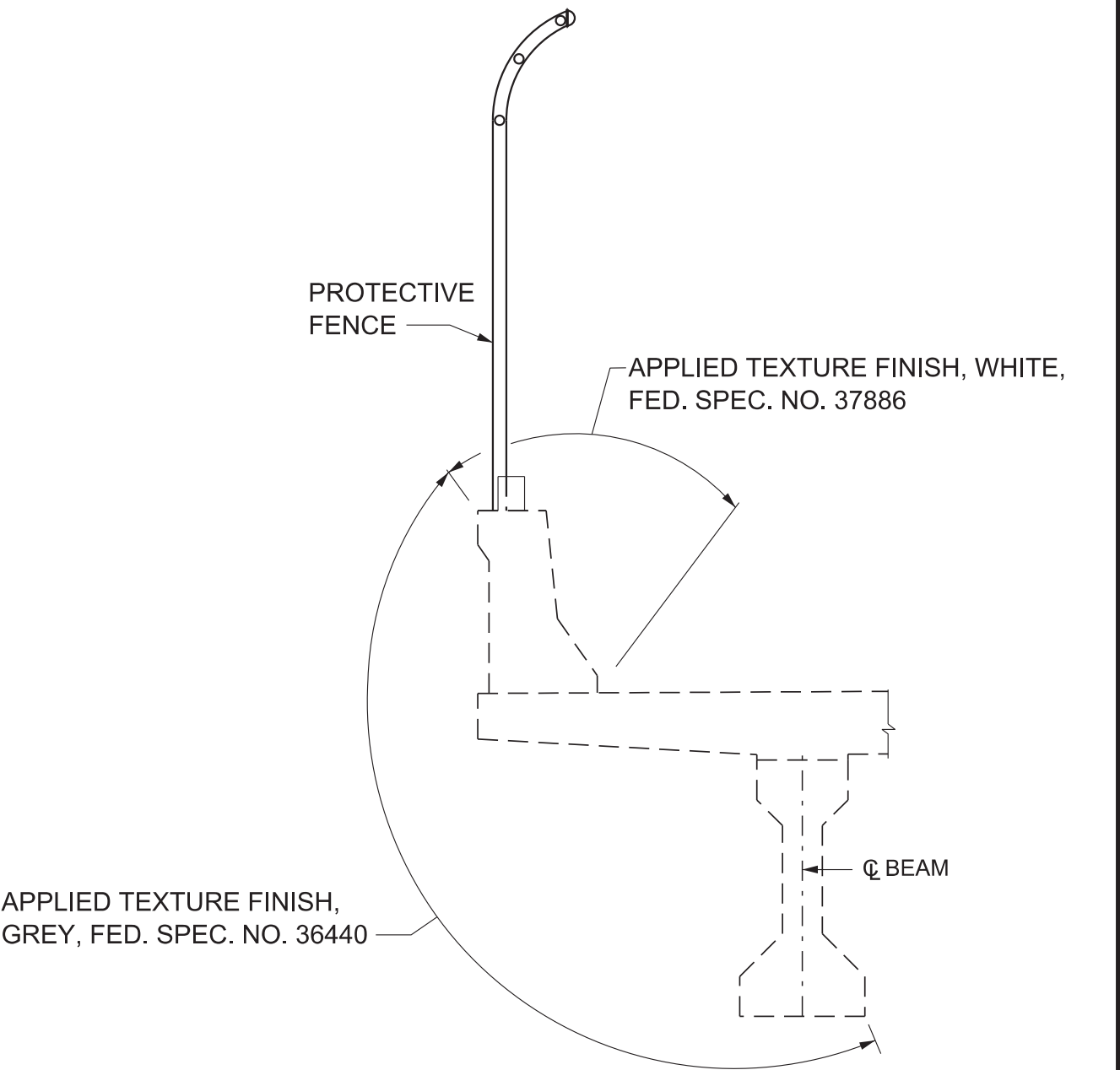
ALL UTILITY INSTALLATIONS OR RELOCATIONS THAT ARE REQUIRED IN CONJUNCTION WITH THIS PROJECT CAN BE INSTALLED OR RELOCATED AS PART OF THE PROJECT PROVIDED THE CONSTRUCTION IS PERFORMED BY THE PROJECT CONTRACTOR OR PROJECT CONTRACTOR'S SUB-CONTRACTOR. HOWEVER, THE UTILITY MUST SUBMIT AN APPLICATION FOR THE INSTALLATION OR RELOCATION TO NS PIPE AND WIRE FOR APPROPRIATE HANDLING FOR LICENSE AGREEMENT AND APPLICABLE FEES. FOR UTILITY APPLICATIONS GO TO:
http://www.nscorp.com/content/nscorp/en/real-estate/norfolk-southern-services/wire-pipeline-fiber-optic-projects.html.
NOTE: LICENSE AGREEMENT MUST BE EXECUTED PRIOR TO UTILITY BEING INSTALLED OR RELOCATED.

NORFOLK SOUTHERN CORP. CONTACT ADDRESSES:

SHAWN STARLING, P.E.
SENIOR ENGINEER PUBLIC IMPROVEMENTS
NORFOLK SOUTHERN RAILWAY COMPANY
ENGINEERING - DESIGN & CONSTRUCTION
650 WEST PEACHTREE STREET NW - BOX 45
ATLANTA, GA 30308
PHONE (470) 463-6721
EMAIL douglas.starling@nscorp.com

PAUL A ANDERSON, DIVISION ENGINEER
1400 NORFOLK SOUTHERN DRIVE
BIRMINGHAM, AL 35210
PHONE (304) 266-1558
EMAIL paul.anderson@nscorp.com

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



APPLIED TEXTURE FINISH SKETCH

LEFT SIDE ONLY SHOWN, SKETCH IS SYMMETRIC ABOUT Ø BRIDGE.

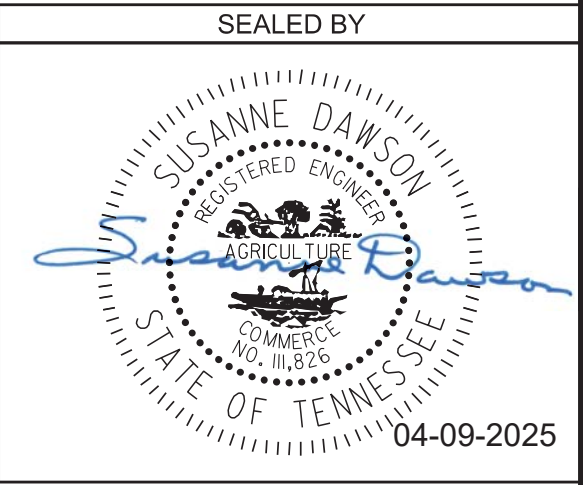
IN ADDITION TO SURFACES SHOWN, ALL EXPOSED CONCRETE SURFACES ON THE ABUTMENT BEAMS, WINGWALLS, BENTS, AND ALL CAP BEAMS AND COLUMNS ARE TO RECEIVE AN APPLIED TEXTURE FINISH, (GREY, AMS-STD-595A, COLOR NO. 36440).

COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 604-04.02.

THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

THE EXISTING SURFACES THAT ARE TO RECEIVE A TEXTURE FINISH SHALL BE FREE OF ALL FLAKING TEXTURE COATING, RUST, DIRT, OIL, AND OTHER FOREIGN SUBSTANCES PRIOR TO THE APPLICATION OF THE TEXTURE FINISH. THE SURFACE SHALL BE CLEANED TO THE COMPLETE SATISFACTION OF THE ENGINEER USING A HIGH PRESSURE WATER WASH. COST TO BE INCLUDED IN ITEM NO. 604-04.02.

THE WASH WATER IS TO BE FILTERED AND PAINT CHIPS AND DEBRIS COLLECTED PRIOR TO RELEASE OF WATER.

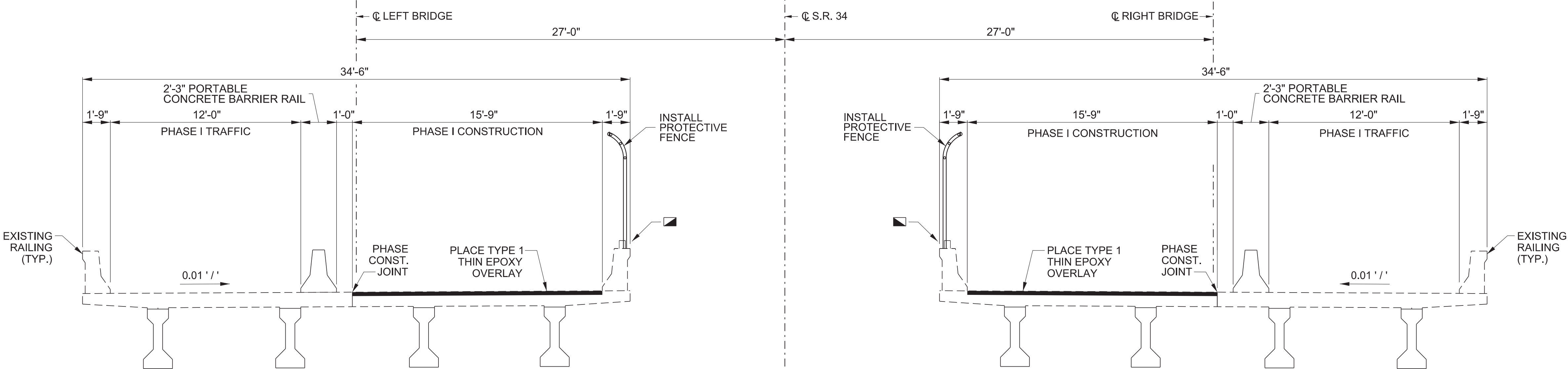


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
GENERAL NOTES AND
ESTIMATED BRIDGE QUANTITIES
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

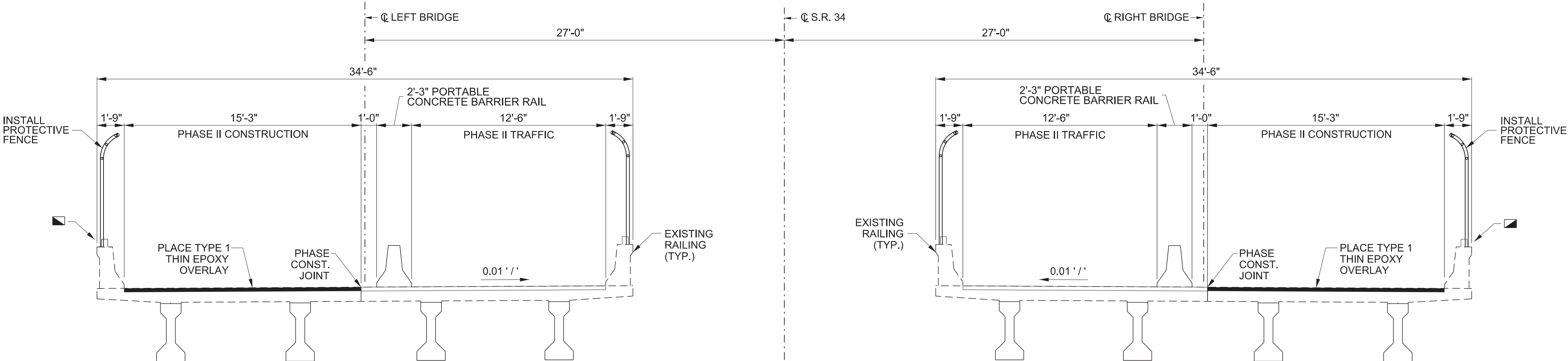
BR-131-903

PIN NO.:	082699.01	
DESIGN BY:	SUSANNE DAWSON	DATE: AUGUST 2021
DRAWN BY:	DONNIE PICKEL	DATE: AUGUST 2021
SUPERVISED BY:	SUSANNE DAWSON	DATE: AUGUST 2021
CHECKED BY:	FRANK BALE	DATE: AUGUST 2021

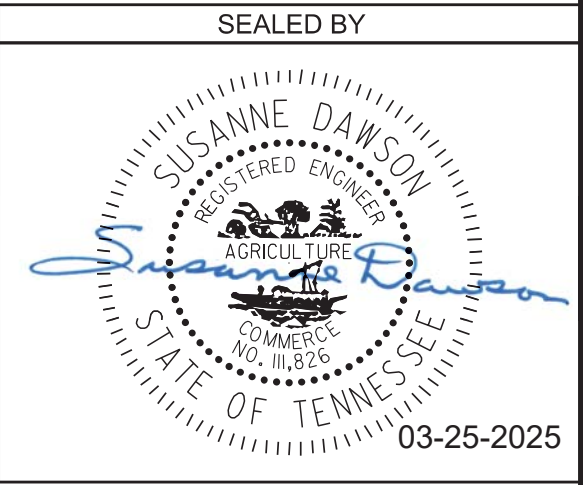
PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



PHASE I CONSTRUCTION
(LOOKING AHEAD ON SURVEY)
☑ INSTALL PARAPET DELINEATORS
(COST TO BE INCLUDED IN ITEMS BID ON)



PHASE II CONSTRUCTION
(LOOKING AHEAD ON SURVEY)
☑ INSTALL PARAPET DELINEATORS
(COST TO BE INCLUDED IN ITEMS BID ON)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-904

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON
DRAWN BY: DONNIE PICKEL
SUPERVISED BY: SUSANNE DAWSON
CHECKED BY: FRANK BALE
DATE: AUGUST 2021
DATE: AUGUST 2021
DATE: AUGUST 2021
DATE: AUGUST 2021

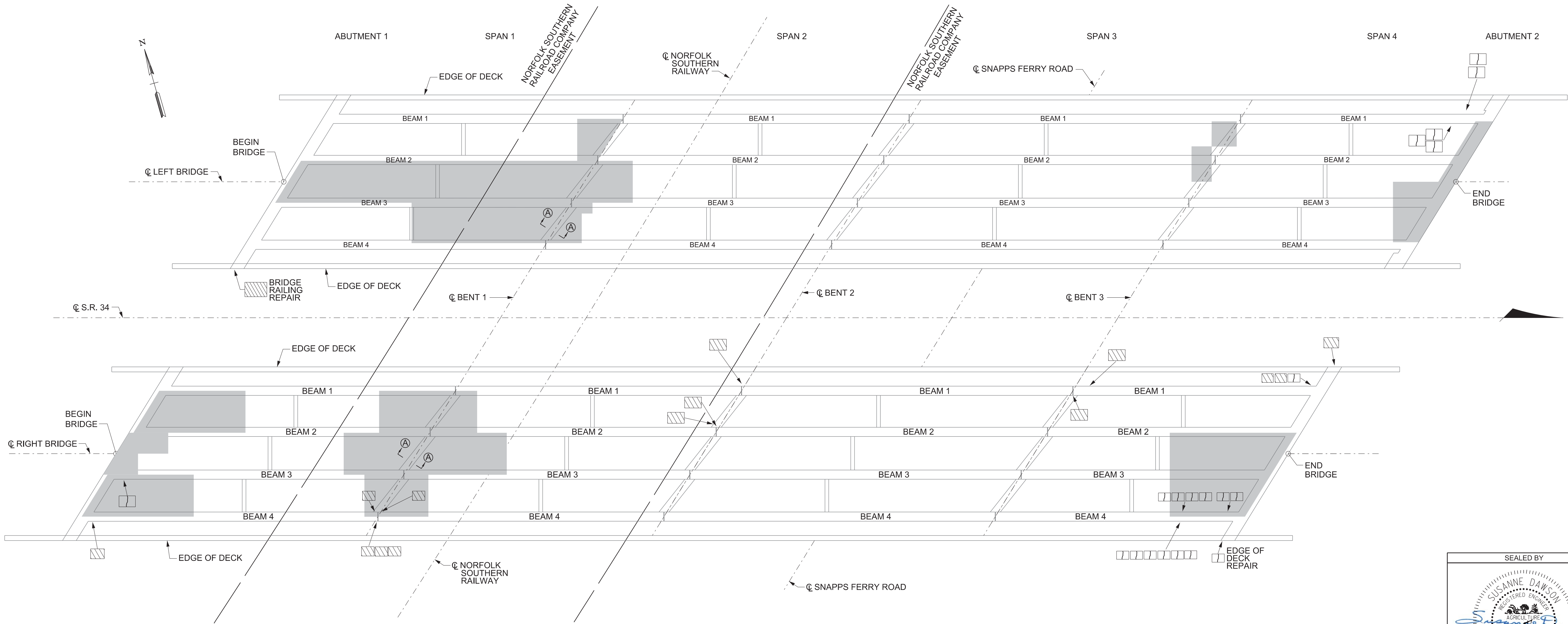
DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-BR-131-915.

DENOTES AREAS OF DECK REPAIR (LOCATIONS ARE FROM TOP OF DECK)

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



PLAN

NOTE: ALL REPAIR LOCATIONS ARE UNDERNEATH OR ON SIDES OF BRIDGES UNLESS OTHERWISE NOTED.

ESTIMATED QUANTITIES				
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.34, CONCRETE OVERLAYED BRIDGE DECK REPAIR (FULL DEPTH)	ITEM NO. 604-10.56, CONCRETE OVERLAYED BRIDGE DECK REPAIR (PARTIAL DEPTH)	ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (S.Y.)	APPROX. REPAIR AREAS (S.Y.)	APPROX. REPAIR AREAS (L.F.)
SUPERSTRUCTURE				
LEFT BRIDGE	10	150	20	19
RIGHT BRIDGE	10	150	20	30

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

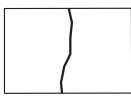
SUPERSTRUCTURE REPAIRS
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

SEALED BY

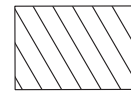


PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON
DRAWN BY: DONNIE PICKEL
SUPERVISED BY: SUSANNE DAWSON
CHECKED BY: FRANK BALE
DATE: AUGUST 2021
DATE: AUGUST 2021
DATE: AUGUST 2021
DATE: AUGUST 2021

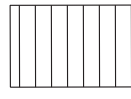
2/14/2025 2:40:49 PM
c:\pw_p11\pickel\p144294646\BR-131-906_WO17_Abutments-LT Bridge.dgn



DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.



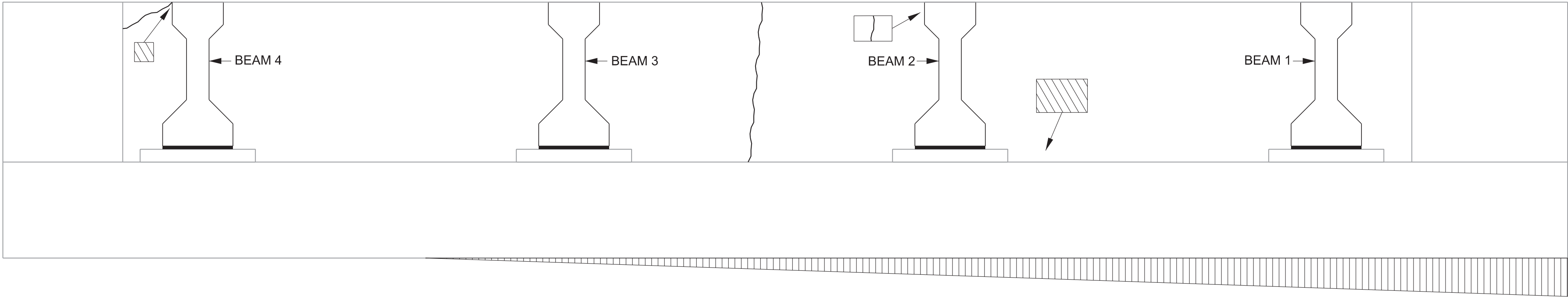
DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.



DENOTES LOCATION TO BE REPAIRED BY PLACEMENT OF CONCRETE UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y. SEE FILL SETTLEMENT DETAIL (THIS SHEET).

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

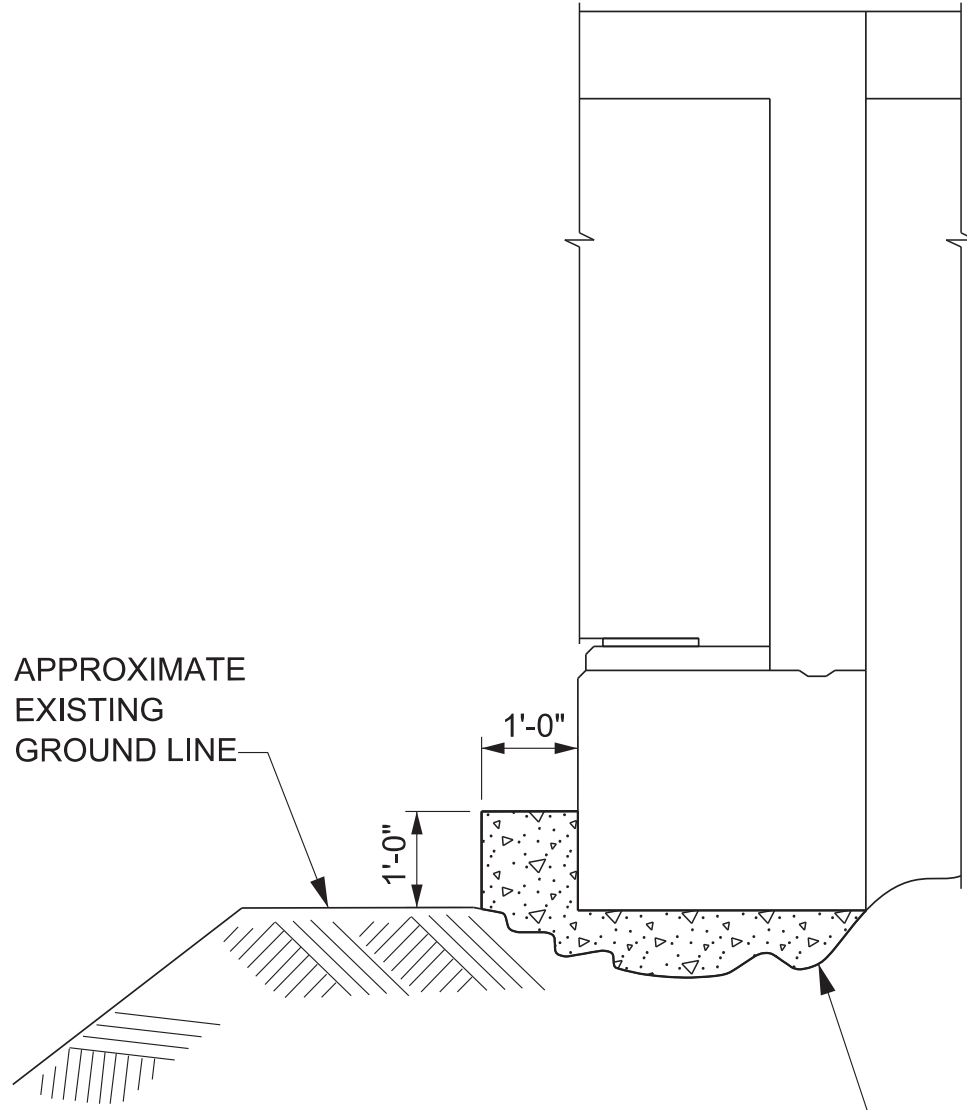
PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



LEFT BRIDGE
ABUTMENT 1 ELEVATION
(LOOKING BACK ON SURVEY)
(NOT TO SCALE)



LEFT BRIDGE
ABUTMENT 2 ELEVATION
(LOOKING AHEAD ON SURVEY)
(NOT TO SCALE)



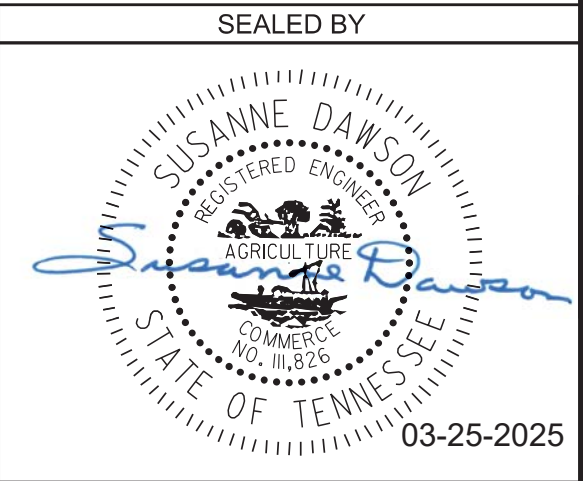
REMOVE LOOSE SOIL. CLASS "A" CONCRETE TO BE PLACED FROM FRONT SIDE OF ABUTMENT, FORMED AND FULLY CONSOLIDATED INTO PLACE.

FILL SETTLEMENT REPAIR DETAIL

ALL COST ASSOCIATED WITH REPAIR OF SETTLEMENT AREA, INCLUDING ALL LABOR AND MISCELLANEOUS MATERIALS, SHALL BE INCLUDED IN ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS),C.Y.

ESTIMATED QUANTITIES			
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.55 CONCRETE (FOUNDATION REPAIRS)	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
LEFT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)	APPROX. REPAIR AREAS (L.F.)
ABUTMENT 1	2	4	6
ABUTMENT 2	5	-	-


PIN NO.:	082699.01	DATE: AUGUST 2021
DESIGN BY:	SUSANNE DAWSON	DATE: AUGUST 2021
DRAWN BY:	DONNIE PICKEL	DATE: AUGUST 2021
SUPERVISED BY:	SUSANNE DAWSON	DATE: AUGUST 2021
CHECKED BY:	FRANK BALE	DATE: AUGUST 2021





STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ABUTMENT REPAIRS
LEFT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025
BR-131-906

2/14/2025 2:42:53 PM
c:\pw_p1\pickel\p1\4294646\BR-131-907_WO17_Abutments-RT Bridge.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021

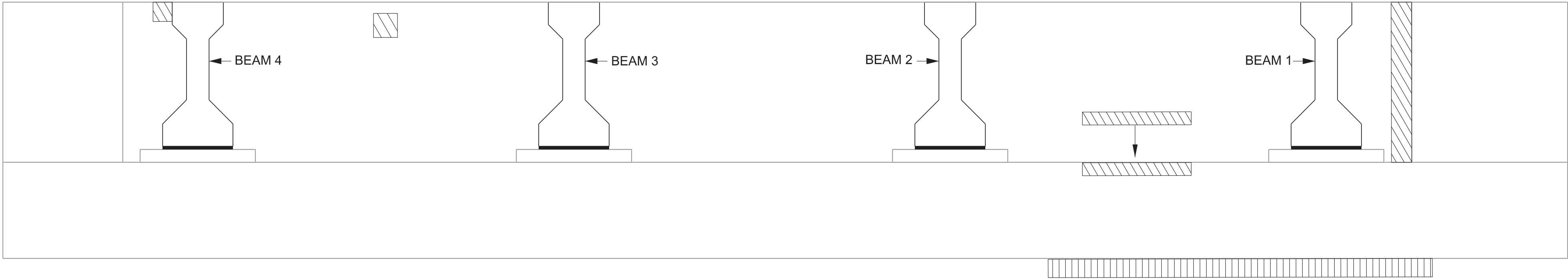
 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

 DENOTES LOCATION TO BE REPAIRED BY PLACEMENT OF CONCRETE UNDER ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS), C.Y. SEE FILL SETTLEMENT DETAIL (THIS SHEET).

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

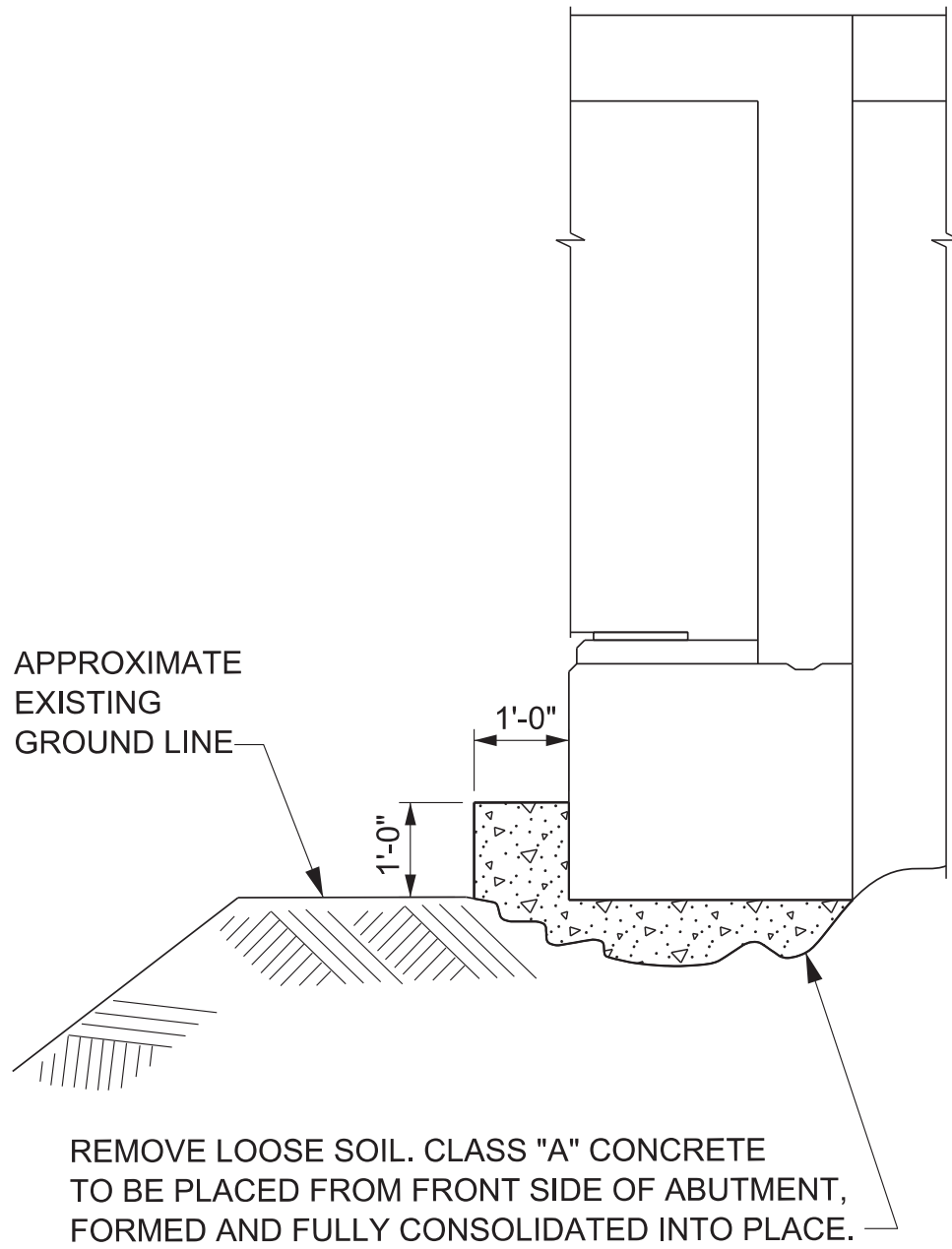
PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



RIGHT BRIDGE
ABUTMENT 1 ELEVATION
(LOOKING BACK ON SURVEY)
(NOT TO SCALE)



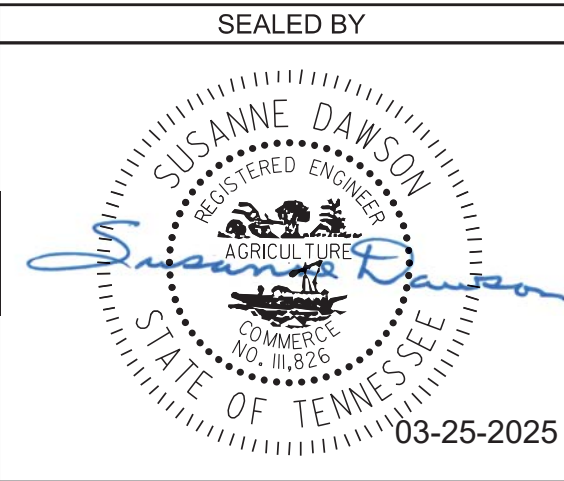
RIGHT BRIDGE
ABUTMENT 2 ELEVATION
(LOOKING AHEAD ON SURVEY)
(NOT TO SCALE)



FILL SETTLEMENT REPAIR DETAIL

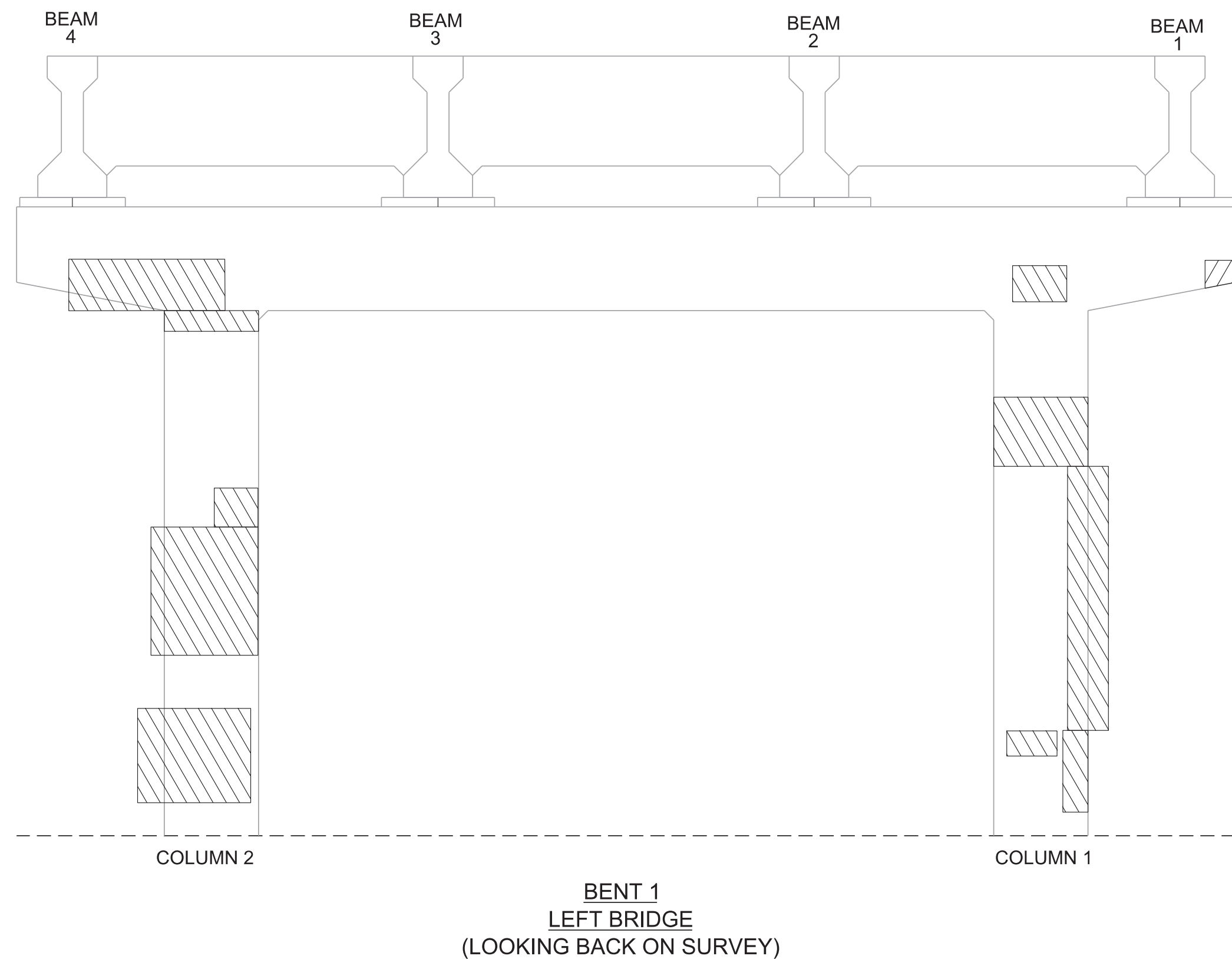
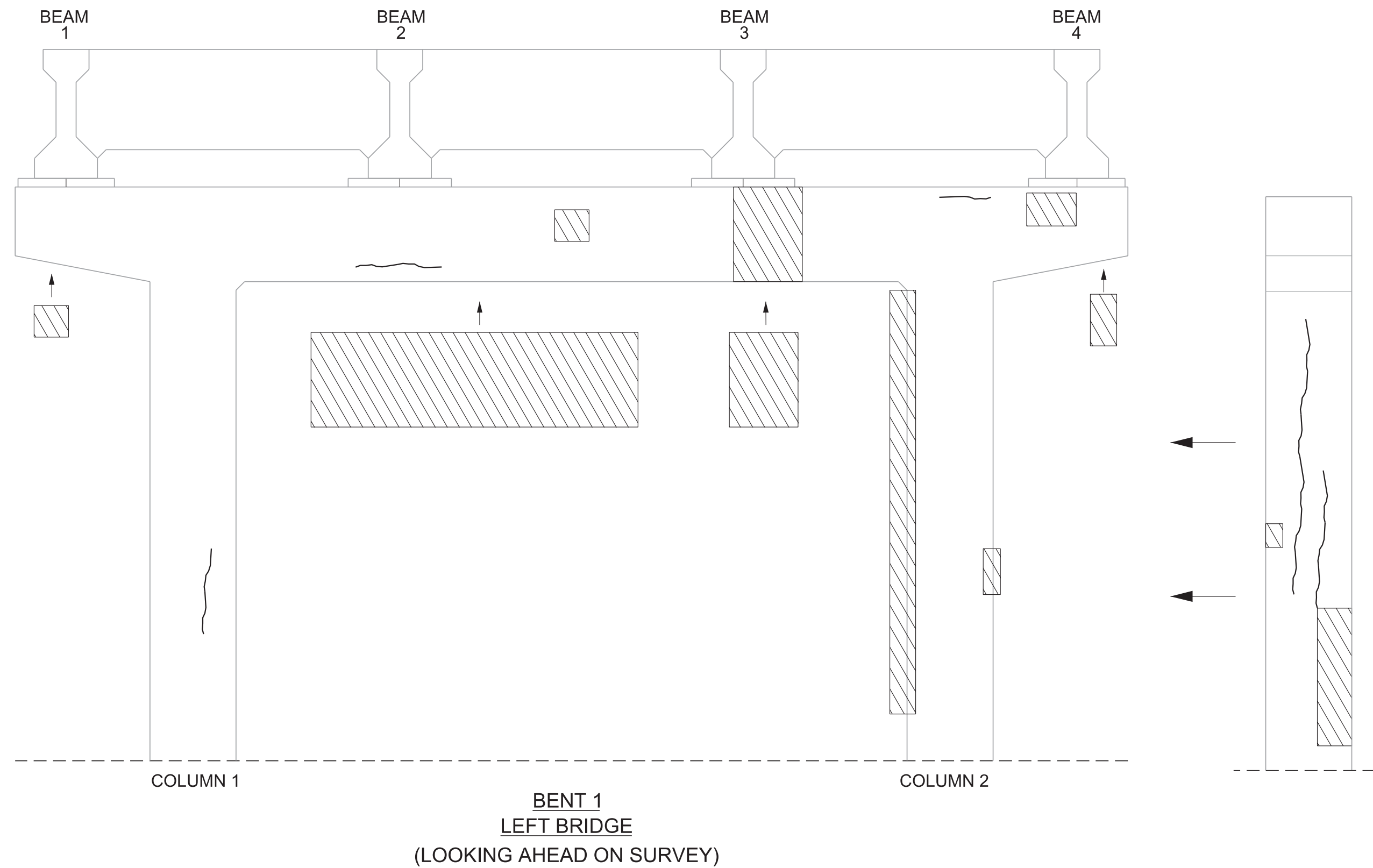
ALL COST ASSOCIATED WITH REPAIR OF SETTLEMENT AREA, INCLUDING ALL LABOR AND MISCELLANEOUS MATERIALS, SHALL BE INCLUDED IN ITEM NO. 604-10.55, CONCRETE (FOUNDATION REPAIRS),C.Y.

ESTIMATED QUANTITIES			
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.55 CONCRETE (FOUNDATION REPAIRS)	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
RIGHT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)	APPROX. REPAIR AREAS (L.F.)
ABUTMENT 1	5	1	-
ABUTMENT 2	1	-	1




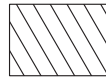
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ABUTMENT REPAIRS
RIGHT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-907



PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

- 

DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.
- 

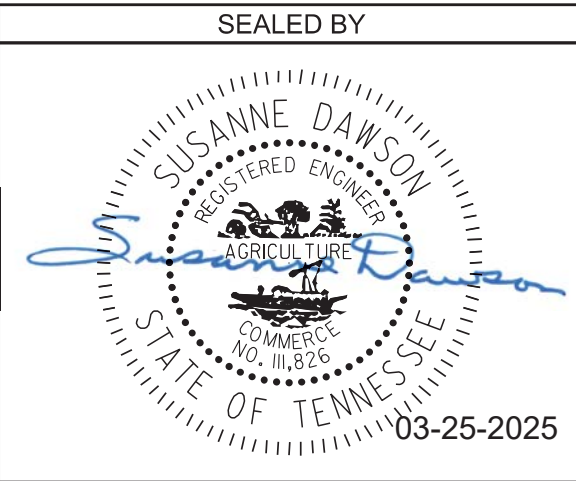
DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
LEFT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 1	97	19

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 1 - LEFT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-908



PIN NO.: 082699.01

DESIGN BY: SUSANNE DAWSON

DRAWN BY: DONNIE PICKEL

SUPERVISED BY: SUSANNE DAWSON

CHECKED BY: FRANK BALE

DATE: AUGUST 2021

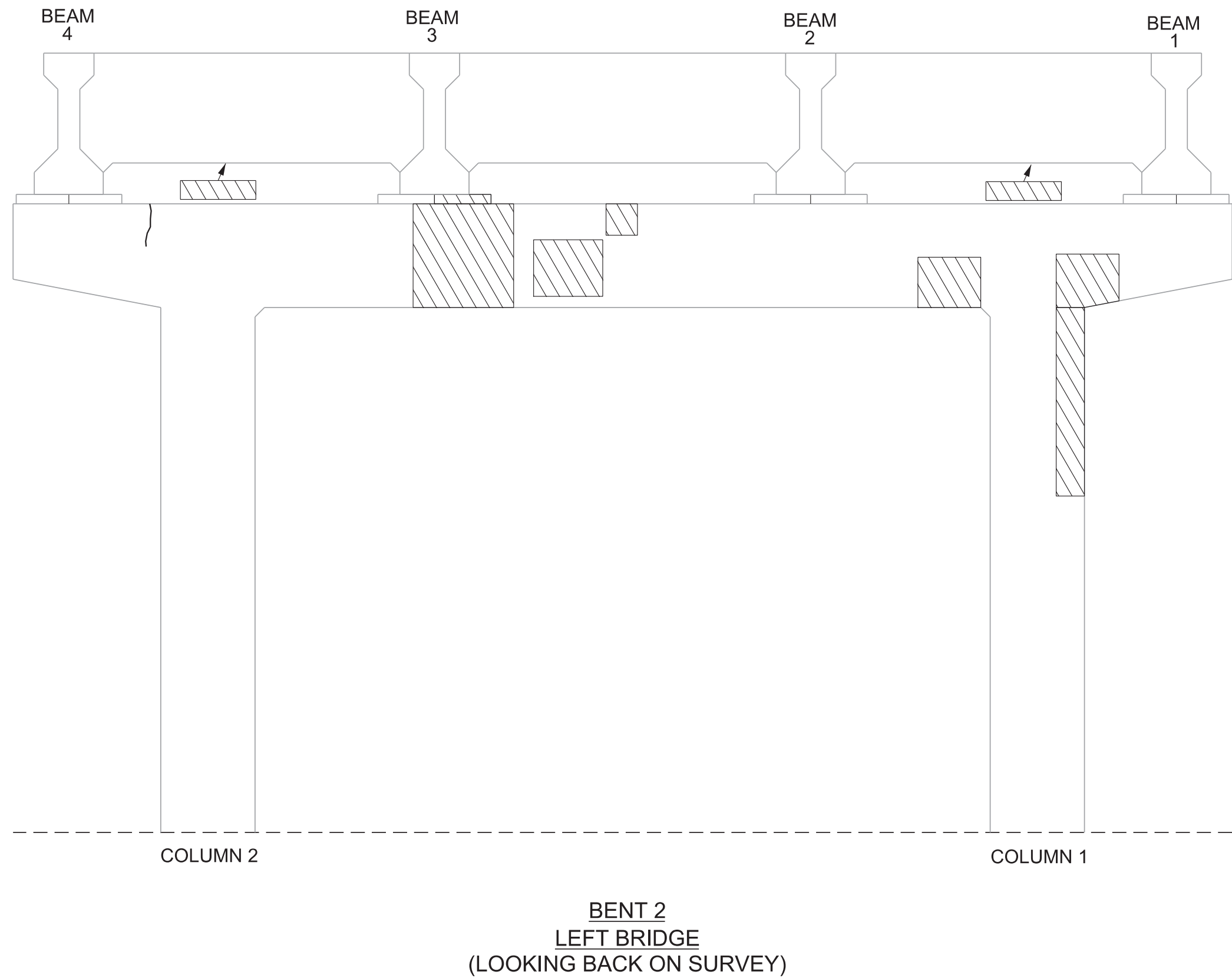
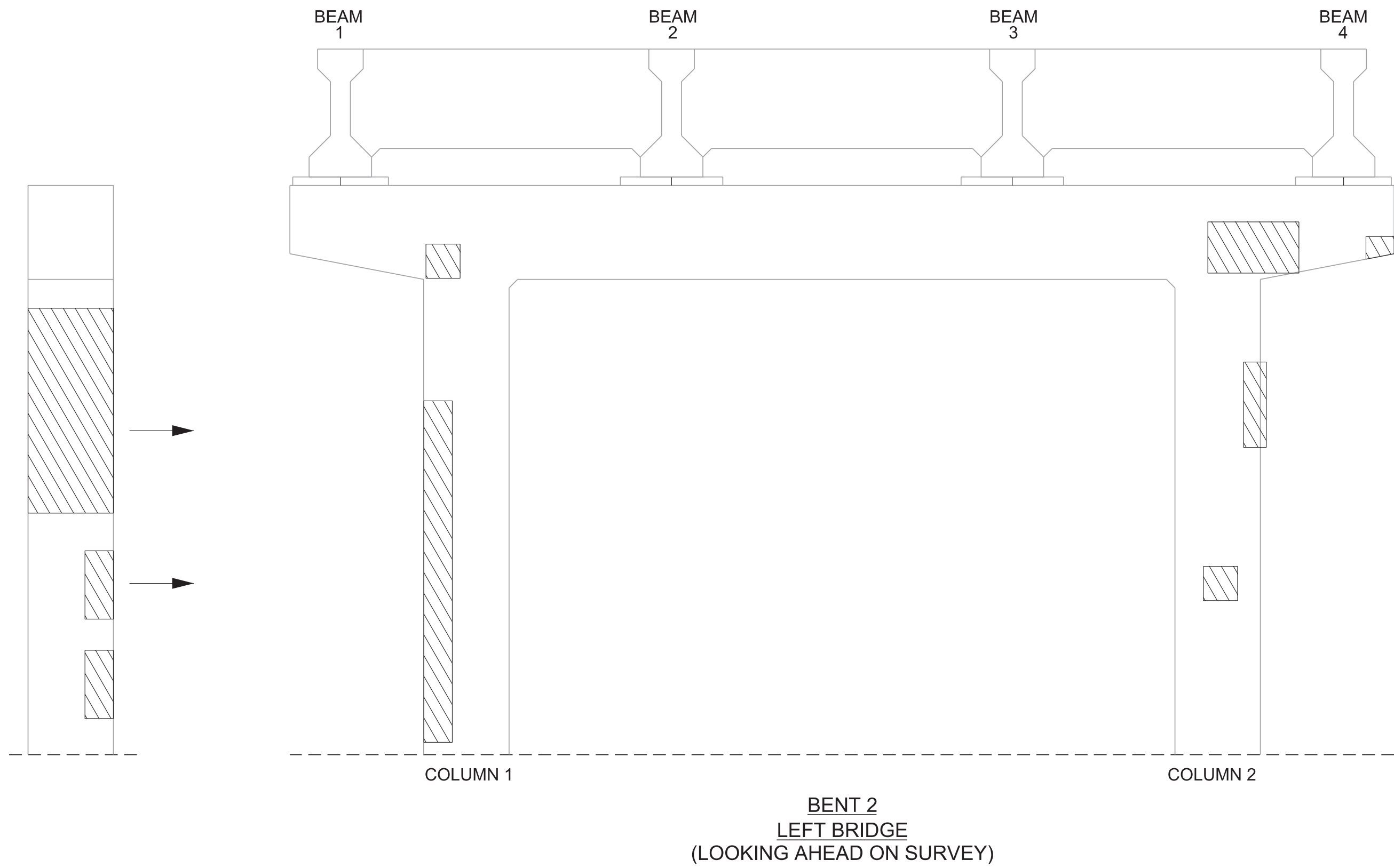
DATE: AUGUST 2021

DATE: AUGUST 2021


DATE: AUGUST 2021


2/14/2025 2:45:46 PM
c:\pw_p1\pickelp\4294646\BR-131-909_WO17_Bent-2-L.T.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021



PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

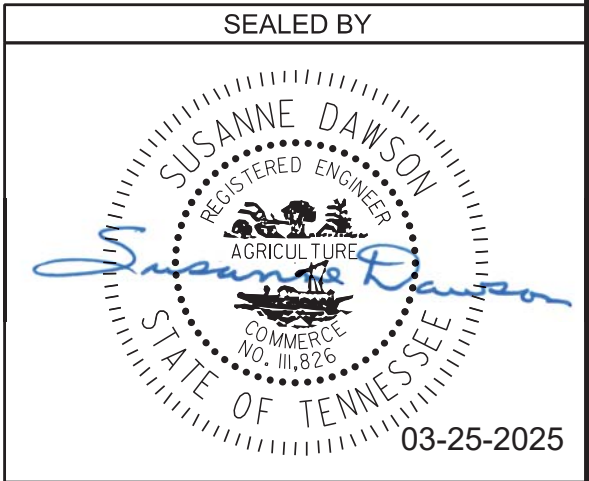
 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

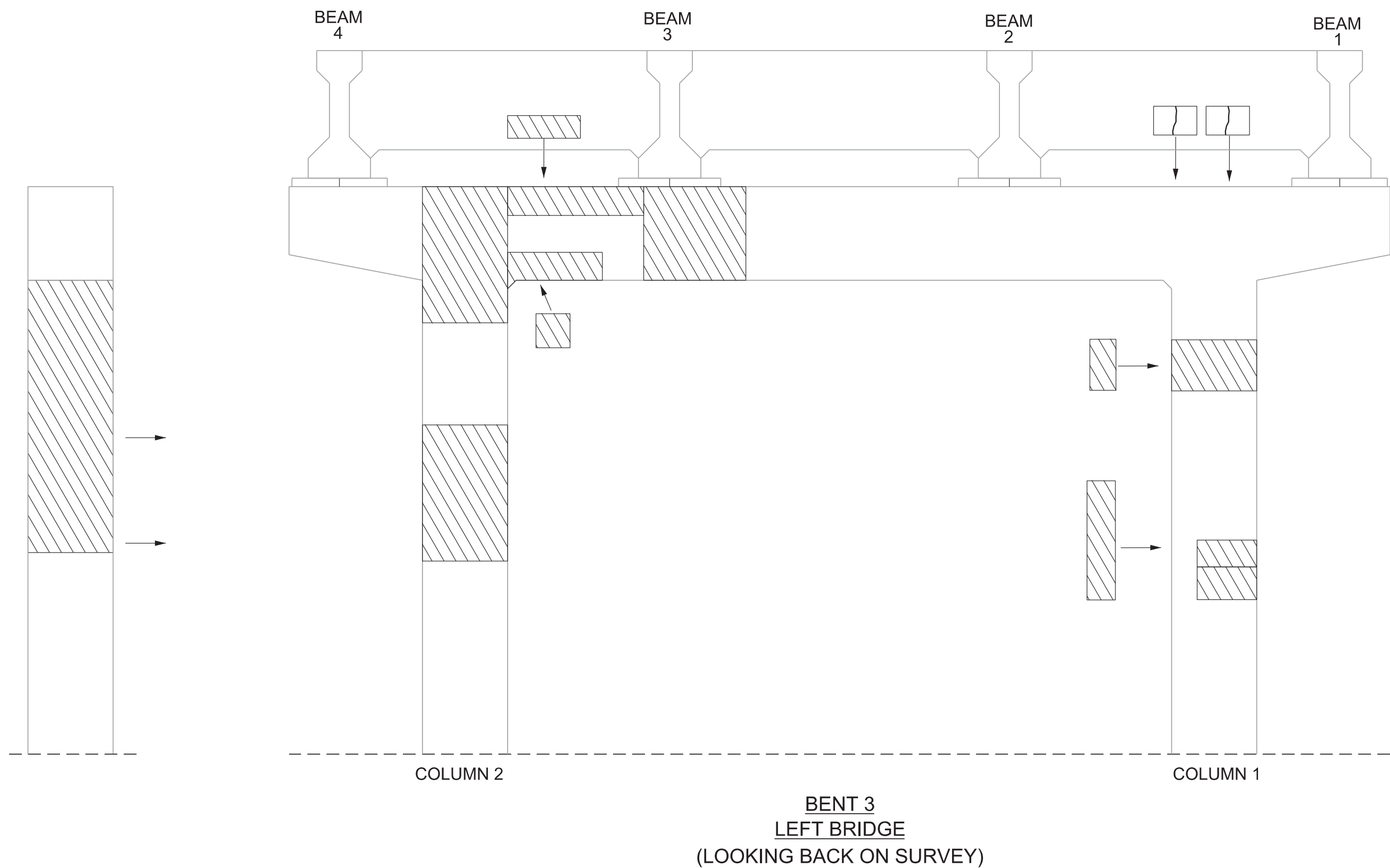
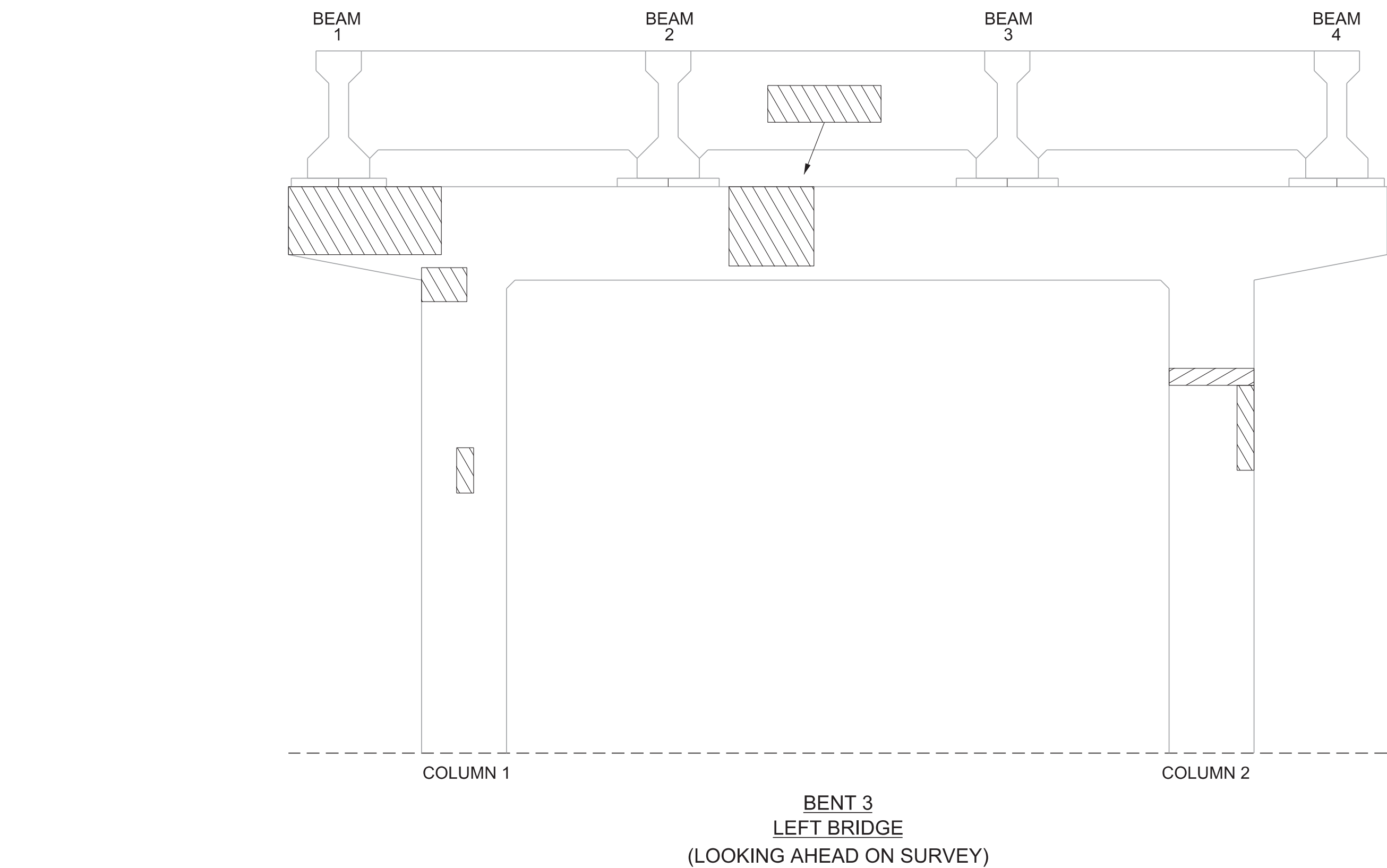
ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
LEFT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 2	55	1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 2 - LEFT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025



2/14/2025 2:46:57 PM
c:\pw_p1\pickelp\4294646\BR-131-910_WO17_Bent-3-LT.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021




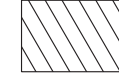
ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
LEFT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 3	91	1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 3 - LEFT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

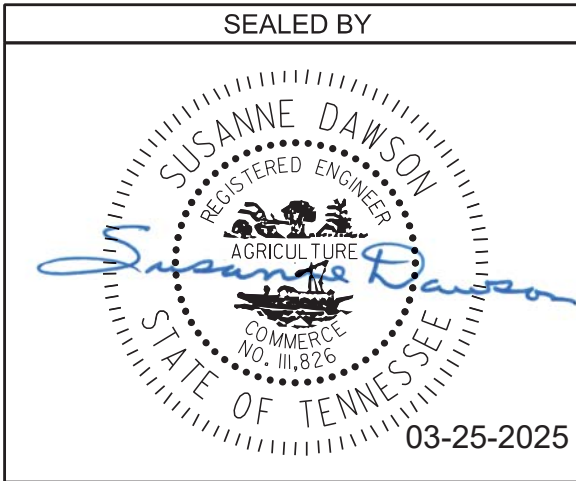
BR-131-910

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

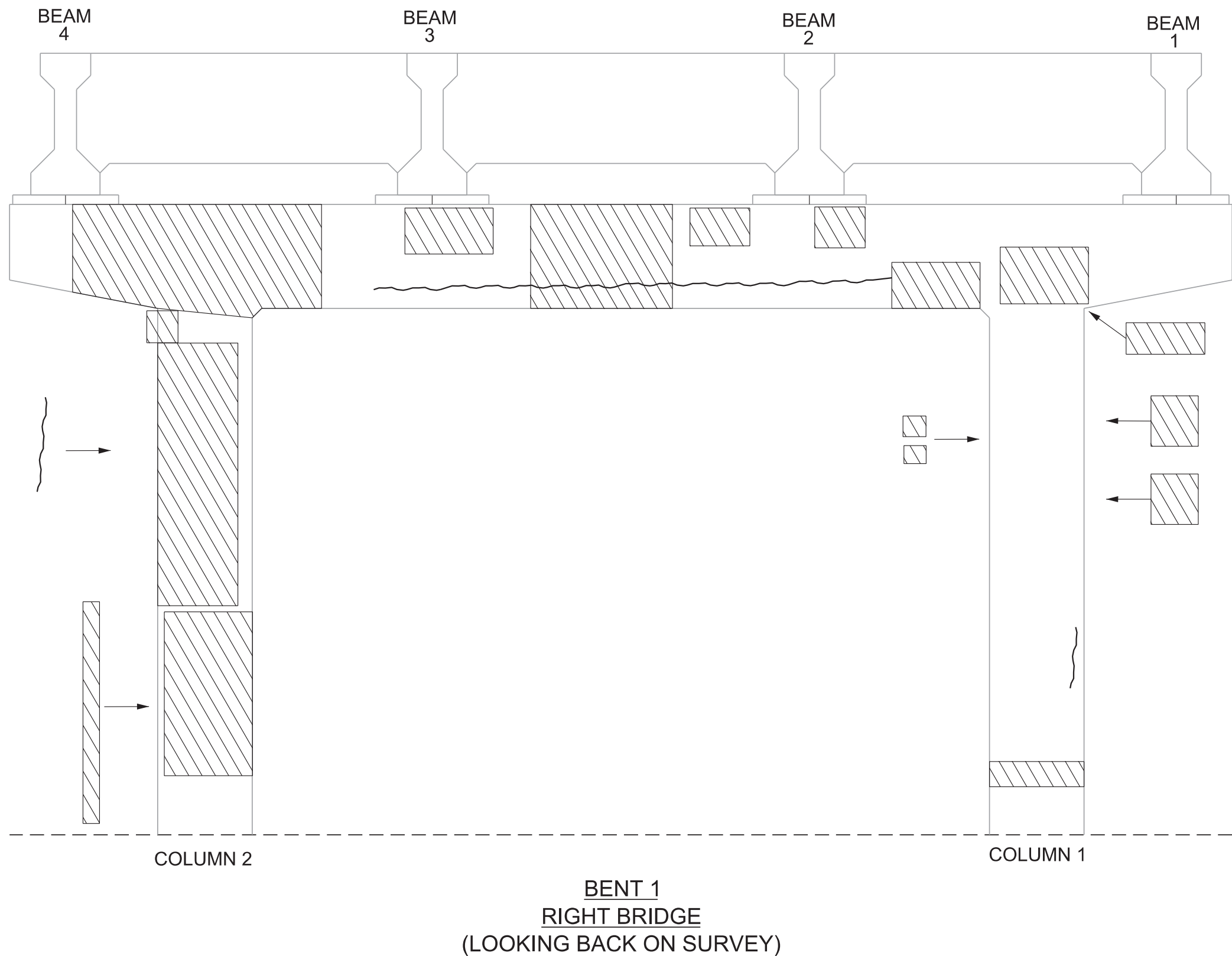
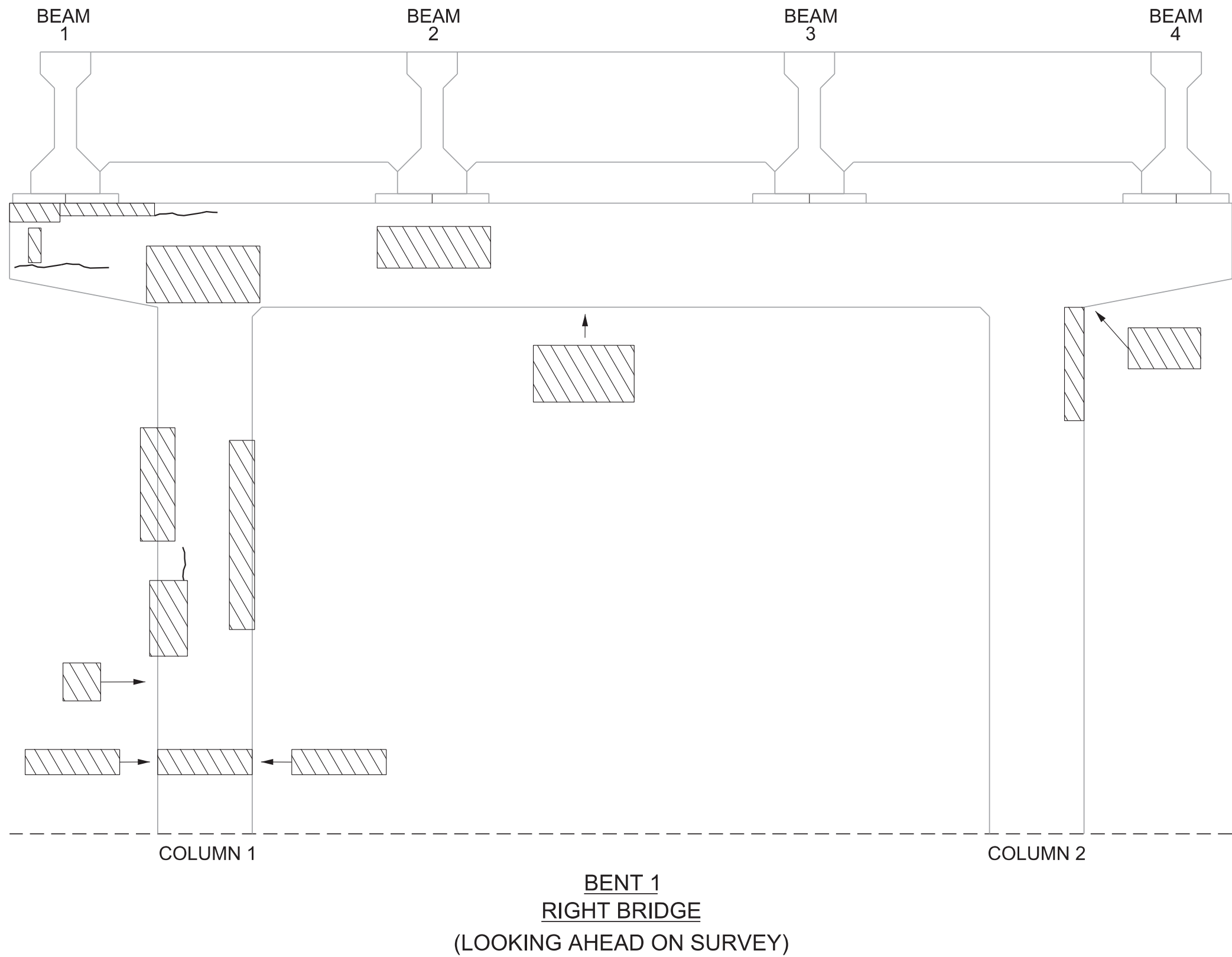
 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.


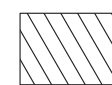


2/14/2025 2:48:20 PM
c:\pw_p11\pickelp\42946\BR-131-911_WO17_Bent1-RT.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021



PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

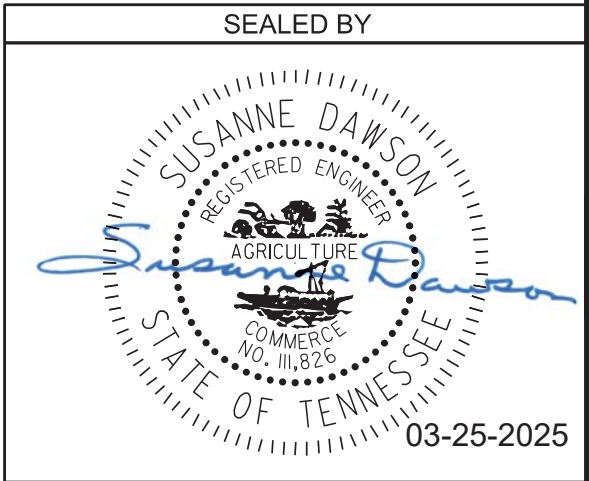
-  DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.
-  DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
RIGHT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 1	96	22

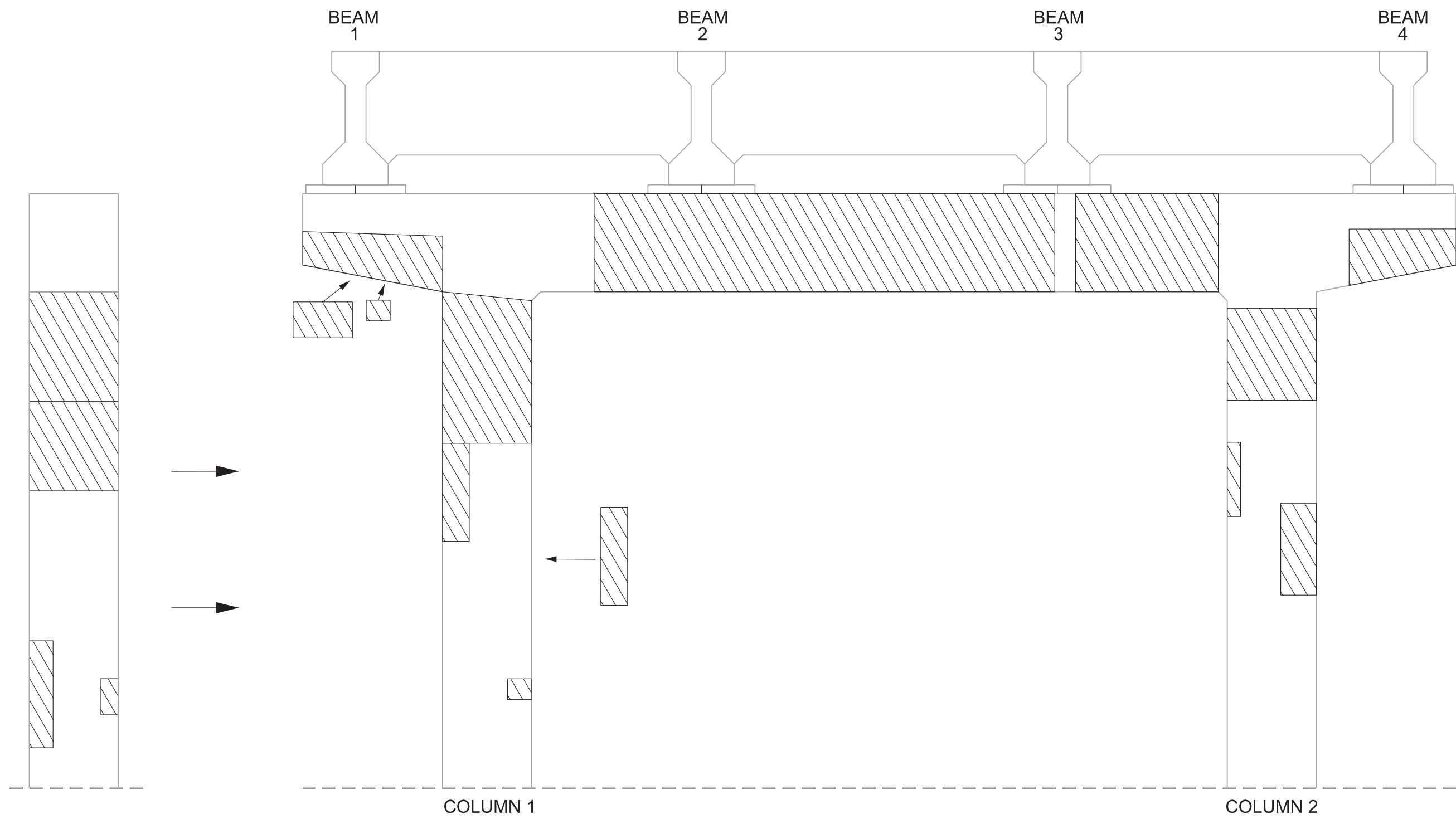
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 1 - RIGHT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-911

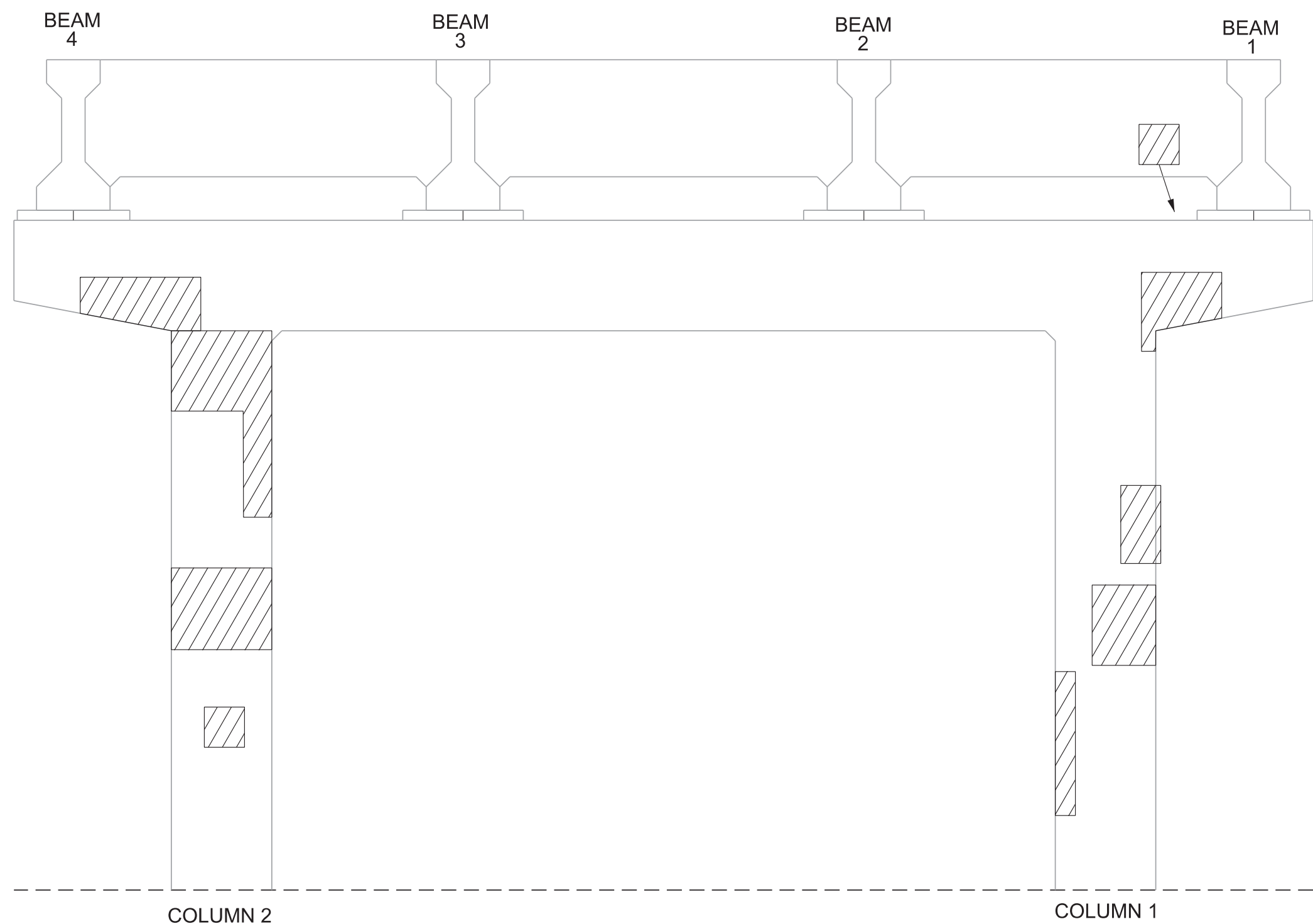


2/14/2025 2:50:16 PM
c:\pw_p1\pickel\p1\4294646\BR-131-912_WO17_Bent-2-RT.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021





BENT 2
RIGHT BRIDGE
(LOOKING AHEAD ON SURVEY)



BENT 2
RIGHT BRIDGE
(LOOKING BACK ON SURVEY)

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

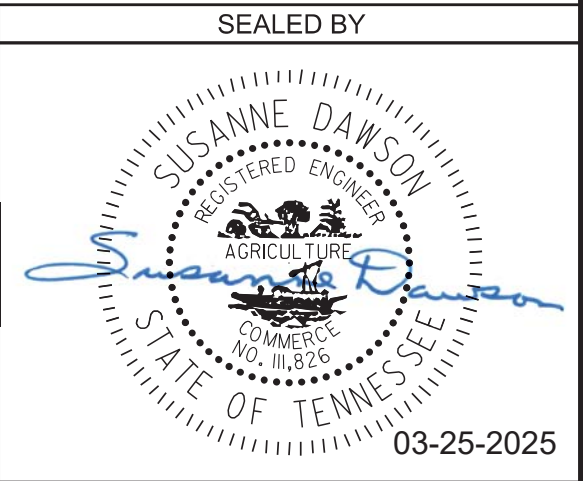
 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
RIGHT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 2	129	0

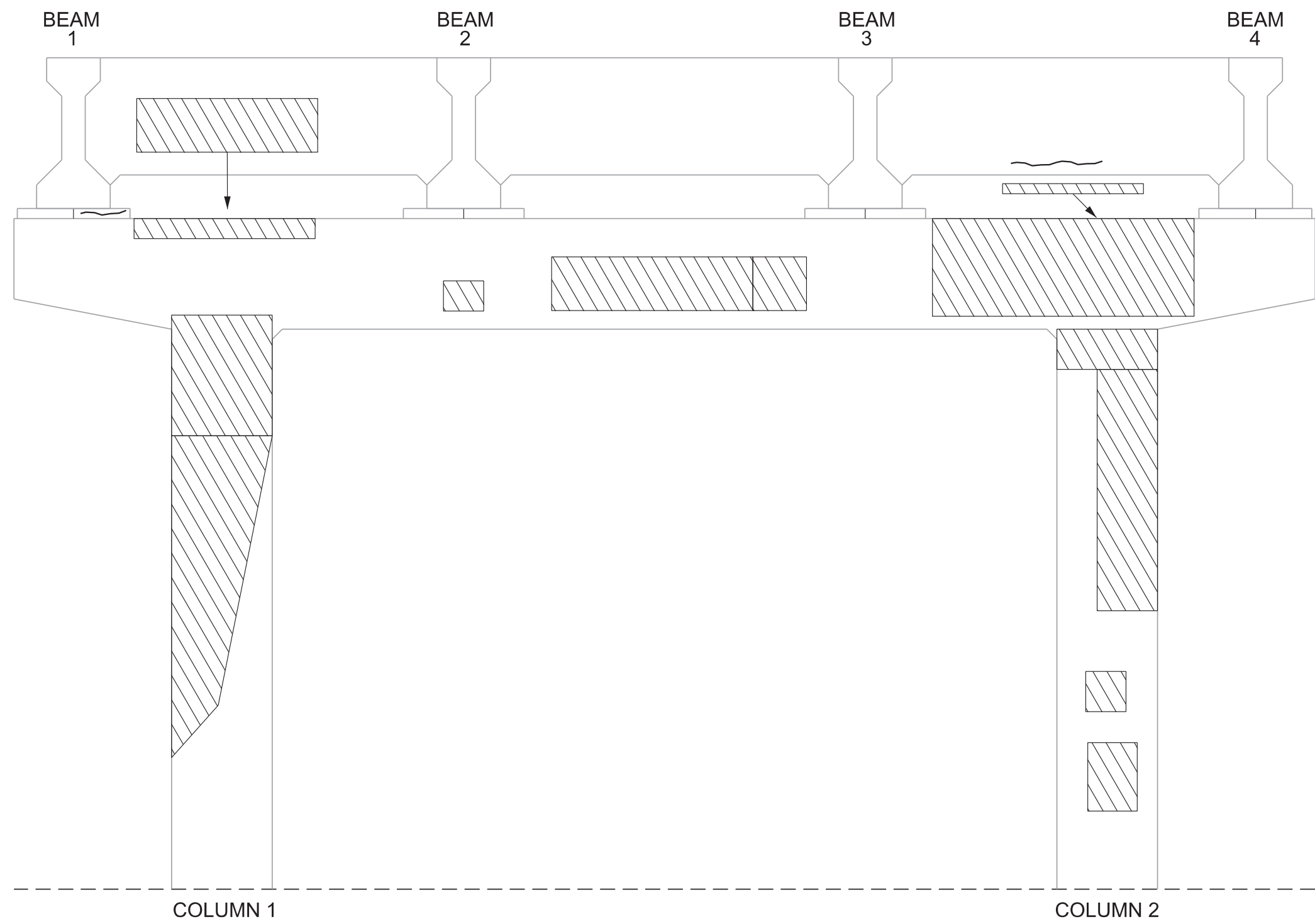
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 2 - RIGHT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-912

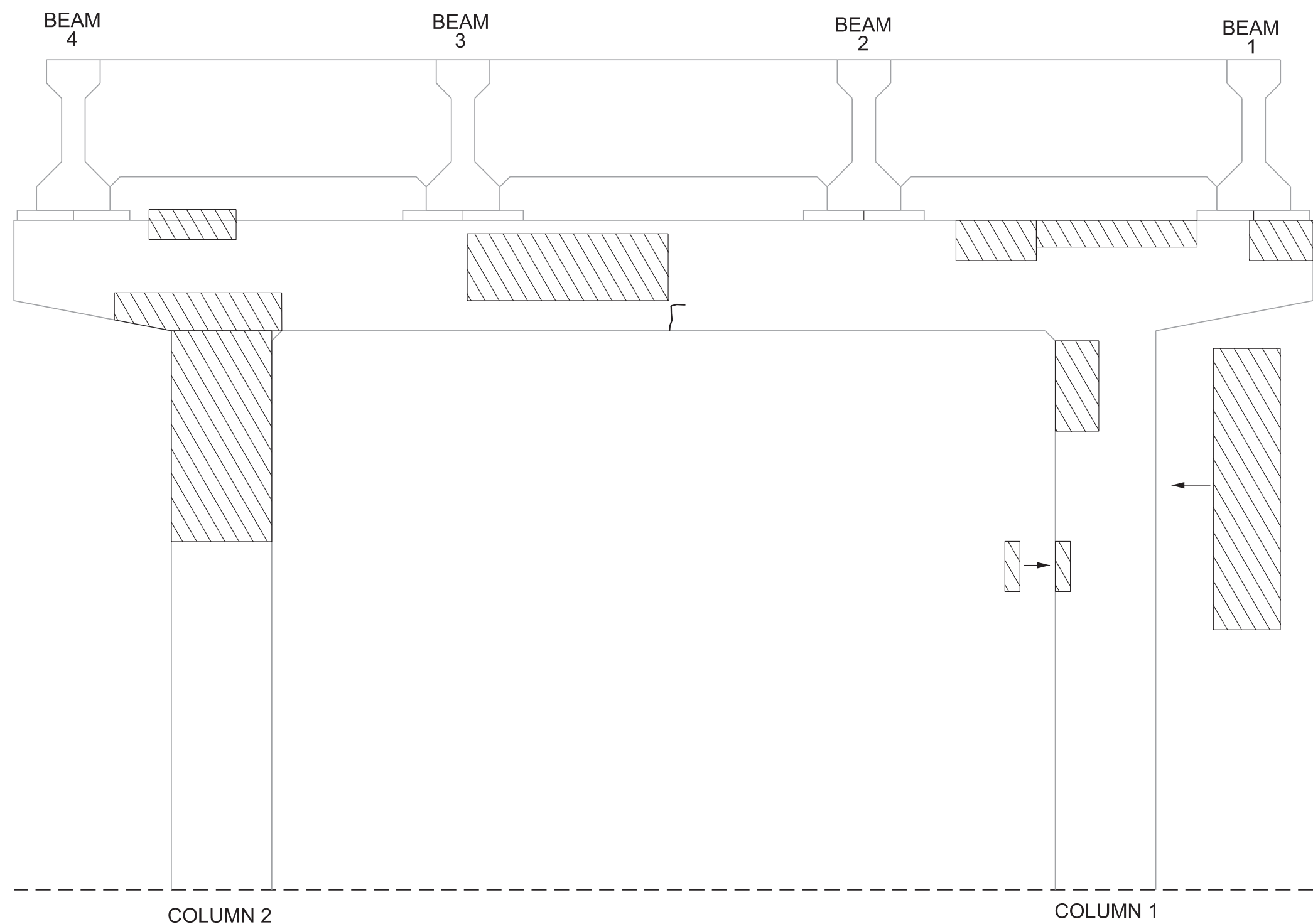


2/14/2025 2:51:18 PM
c:\pw_p11\pickel\p14294646\BR-131-913_WO17_Bent-3-RT.dgn

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021





BENT 3
RIGHT BRIDGE
(LOOKING AHEAD ON SURVEY)



BENT 3
RIGHT BRIDGE
(LOOKING BACK ON SURVEY)

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-131-915.

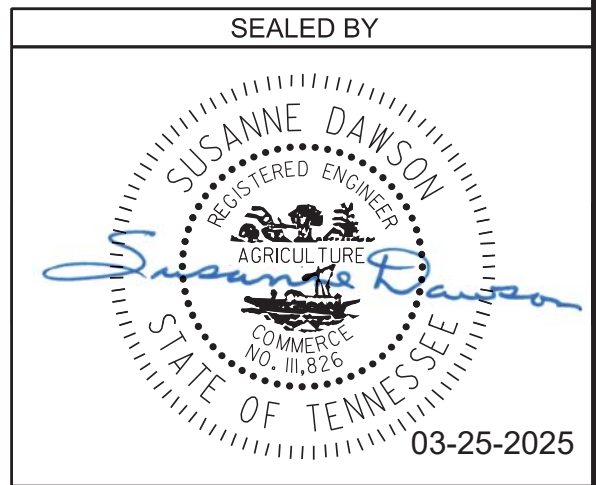
 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54. SEE REPAIR DETAILS ON DWG. NO. BR-131-915.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

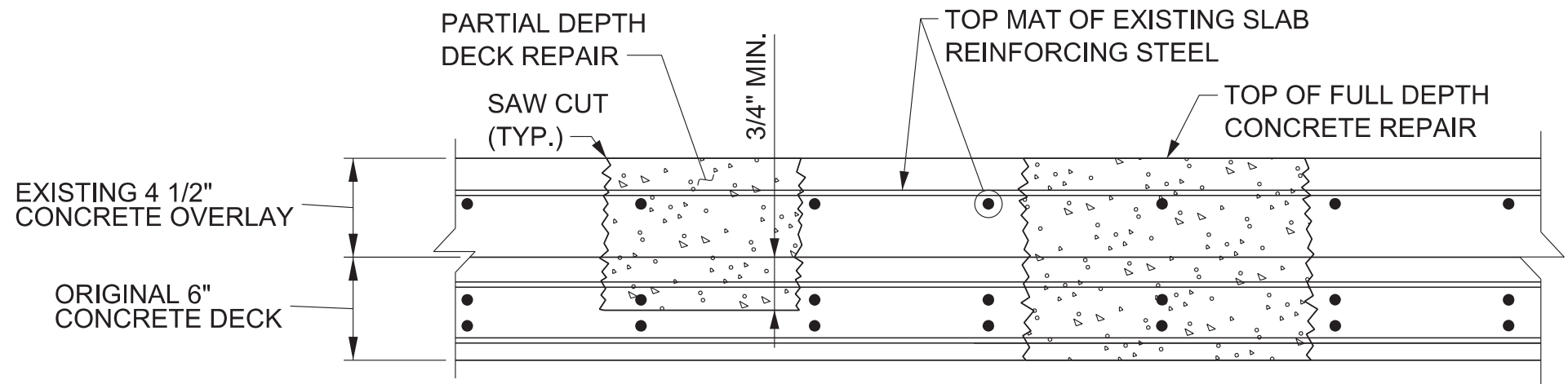
ESTIMATED QUANTITIES		
LOCATION OF REPAIR	ITEM NO. 604-10.05, CONCRETE, AND/OR 604-10.54, CONCRETE REPAIRS	ITEM NO. 604-10.62 EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)
RIGHT BRIDGE	APPROX. REPAIR AREAS (S.F.)	APPROX. REPAIR AREAS (L.F.)
BENT 3	110	4

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT REPAIRS
BENT 3 - RIGHT BRIDGE
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-913



2/14/2025 2:52:14 PM
c:\pw_p1\picldp\04294646\BR-131-914_WO17_RepairDetails.dgn



DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

CONCRETE FOR FULL AND PARTIAL DEPTH DECK REPAIRS SHALL BE HIGH EARLY STRENGTH CONCRETE WITH A COMPRESSIVE STRENGTH OF 3,500 P.S.I. @ 28 DAYS. TRAFFIC SHALL NOT BE PERMITTED ON ANY REPAIR AREA UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. MINIMUM AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.

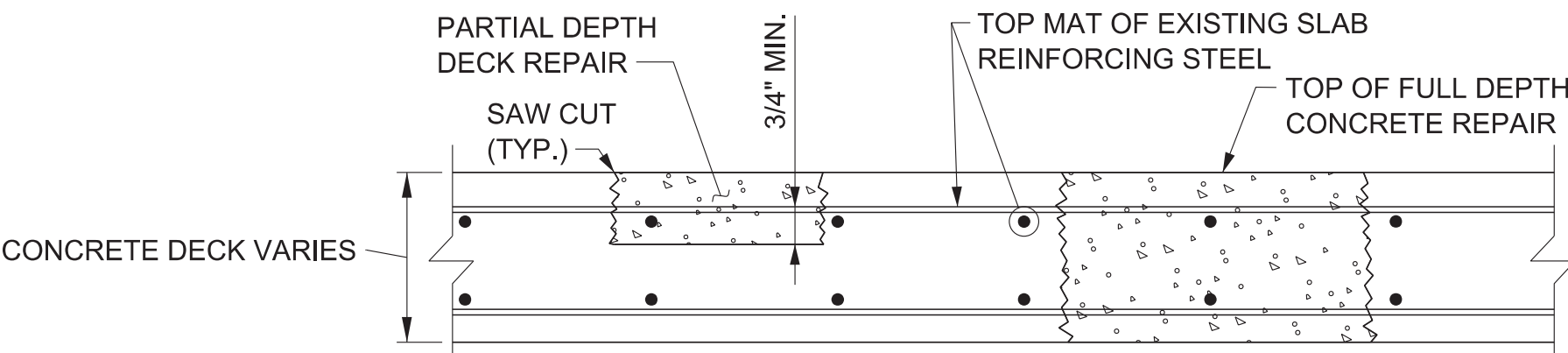
REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF THE ORIGINAL SLAB REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.

DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.56, CONCRETE OVERLAYED BRIDGE DECK REPAIR (PARTIAL DEPTH), AND ITEM NO.604-10.34, CONCRETE OVERLAYED BRIDGE DECK REPAIR (FULL DEPTH). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.34. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- 1) (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED.
- 2) (FULL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 POUND CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS.
- 3) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

ITEM NOS. 604-10.34 AND 604-10.56 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

EXTREME CARE SHALL BE TAKEN WHEN REMOVING DETERIORATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.



DETAILS SHOWING FULL AND PARTIAL DEPTH DECK REPAIR
OVERHANGS OF LEFT AND RIGHT BRIDGES

CONCRETE FOR FULL AND PARTIAL DEPTH DECK REPAIRS SHALL BE HIGH EARLY STRENGTH CONCRETE WITH A COMPRESSIVE STRENGTH OF 3,500 P.S.I. @ 28 DAYS. TRAFFIC SHALL NOT BE PERMITTED ON ANY REPAIR AREA UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. MINIMUM AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.

REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF THE ORIGINAL SLAB REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.

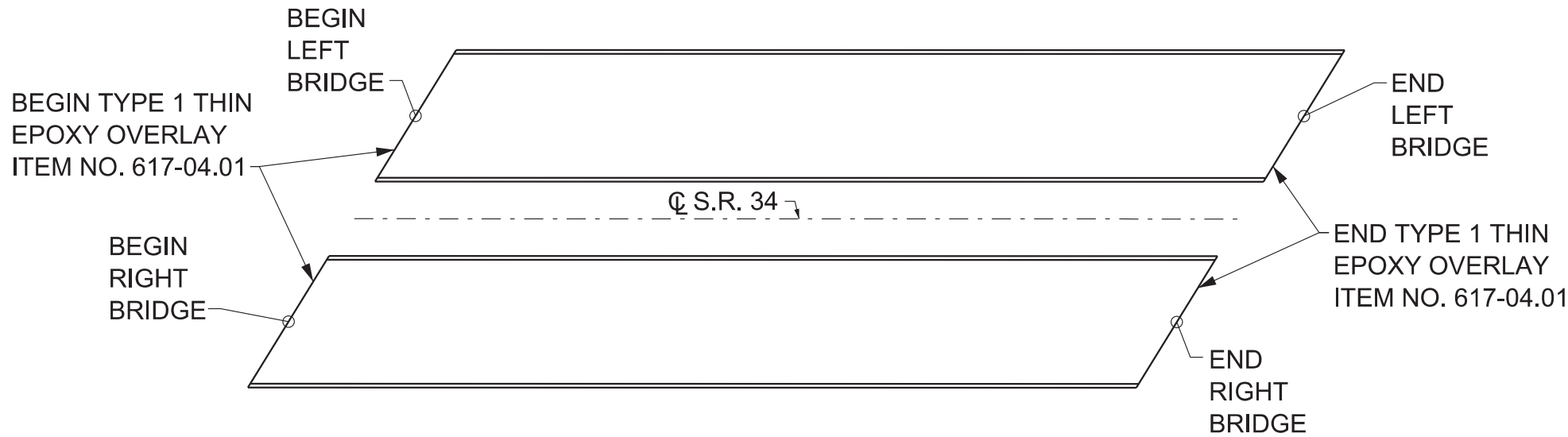
DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB), AND 604-10.30, BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.30. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- 1) (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED.
- 2) (FULL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 POUND CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS.
- 3) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

ITEM NOS. 604-10.30 AND 604-10.50 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

EXTREME CARE SHALL BE TAKEN WHEN REMOVING DETERIORATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.

PIN NO.: 082699.01
DESIGN BY: SUSANNE DAWSON DATE: AUGUST 2021
DRAWN BY: DONNIE PICKEL DATE: AUGUST 2021
SUPERVISED BY: SUSANNE DAWSON DATE: AUGUST 2021
CHECKED BY: FRANK BALE DATE: AUGUST 2021



PLAN - THIN EPOXY OVERLAY

THIN EPOXY OVERLAY NOTES

TYPE 1 THIN EPOXY OVERLAY SYSTEM USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED. THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

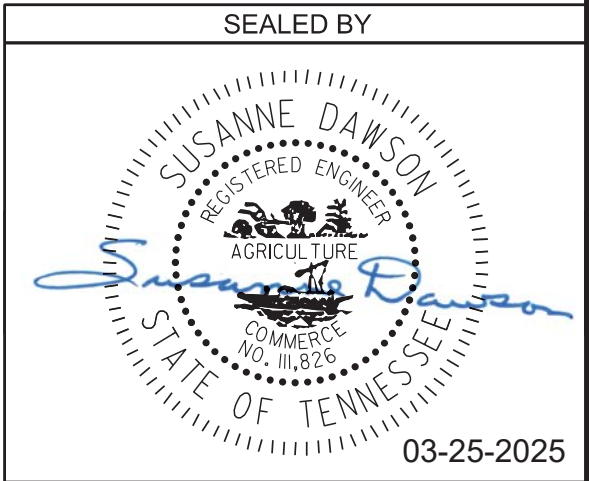
THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS
BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

BR-131-914



SPECIAL NOTES FOR EPOXY INJECTION

UNLESS OTHERWISE NOTED, THE INTENT OF THIS SPECIFICATION IS FOR DESIGNATED CRACKS TO BE INJECTED THEIR FULL LENGTH AND DEPTH.

DESIGNATED CRACKS SHALL BE INJECTED WITH AN APPROVED EPOXY RESIN ADHESIVE FILLING ALL VOIDS FOR THE CRACK DEPTH OR THICKNESS OF THE MEMBER. THE EPOXY RESIN ADHESIVE SHALL BE ON THE CURRENT QUALIFIED PRODUCTS LIST MAINTAINED BY THE DIVISION OF MATERIALS AND TEST. ALL CRACKS SHALL BE INJECTED USING AN ADHESIVE SUITABLE FOR THE FIELD CONDITIONS (CRACK WIDTH, TEMPERATURE, HUMIDITY, ETC.) RECOMMENDED BY THE ADHESIVE MANUFACTURER AS SHOWN ON MATERIAL DATA SHEETS. FOLLOWING INJECTION, ALL INJECTION PORTS AND CAPPING MATERIAL SHALL BE REMOVED FROM EXPOSED SURFACES LEAVING THE SURFACE SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACES.

THE CONTRACTOR SHALL HAVE SUFFICIENT EXPERIENCE AND TRAINING TO PERFORM THE EPOXY INJECTION IN ACCORDANCE WITH THESE PLANS. PRIOR TO PERFORMING ANY WORK, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A WRITTEN PROCEDURE FOR PERFORMING THE EPOXY INJECTION. THE PROCEDURE SHALL DESCRIBE IN DETAIL HOW THE WORK WILL BE PERFORMED. THE PROCEDURE SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING INFORMATION.

- 1) DESCRIPTION OF EQUIPMENT.

A. THE INJECTION EQUIPMENT SHALL BE OF THE TYPE THAT MIXES ADHESIVE COMPONENTS AT THE INJECTION HEAD.

B. THE INJECTION EQUIPMENT SHALL BE CAPABLE OF DISCHARGING MIXED ADHESIVE AT ANY PRESSURE UP TO 300 PSI. THE INJECTION EQUIPMENT SHALL BE EQUIPPED WITH GAUGES WHICH CAN MEASURE THE INJECTION PRESSURE AND VOLUME.
- 2) EQUIPMENT CALIBRATION PROCEDURES AND SCHEDULE.
- 3) MATERIALS TO BE USED (INCLUDING MANUFACTURER DATA SHEETS)

A. CAPPING MATERIAL

B. EPOXY ADHESIVE (TYPE TO BE APPROPRIATE FOR CRACK SIZES TO BE INJECTED).
- 4) PORT SPACING

A. PORT SPACING SHALL NOT BE LESS THAN THE THICKNESS OF THE CONCRETE IN THAT LOCATION.
- 5) INJECTION SEQUENCE

A. INJECTION SHALL PROCEED FROM LOWER END OF CRACK ALONG ADJACENT PARTS.

B. SKIPPING OF PORTS DURING INJECTION SHALL NOT BE ALLOWED.

THE CONTRACTOR SHALL HAVE THE MANUFACTURER'S INSTRUCTIONS FOR PROPORTIONING AND MIXING AVAILABLE AT THE JOB SITE AT ALL TIMES AND SHALL ENSURE THAT THE EQUIPMENT IS SUPPLYING THE MIXED ADHESIVE IN THE CORRECT PROPORTIONS.

TO ENSURE PROPER MIXING AND PROPORTIONING, SAMPLES SHALL BE TAKEN FROM THE INJECTOR HEAD AT THE START OF EACH WORKDAY AND EACH TIME THE ADHESIVE RESERVOIRS ARE REFILLED. THE SAMPLES SHALL BE IN A TEST CUP. THE SAMPLE SHALL BE MONITORED TO ENSURE THAT THE CURE TIME IS IN COMPLIANCE WITH THE MANUFACTURER'S DATA SHEETS. IF THE SAMPLES DO NOT CURE IN THE SPECIFIED TIME THEN THE EQUIPMENT USED TO PRODUCE THE SAMPLE SHALL NOT BE USED UNTIL THE PROBLEM IS CORRECTED.

CORE SAMPLES SHALL BE TAKEN AS VERIFICATION OF THE QUALITY OF WORK. THE CONTRACTOR SHALL TAKE ONE (1) ONE (1) INCH DIAMETER (FULL DEPTH OF CONCRETE AT LOCATION CORED) CORE SAMPLES STARTING WITH THE FIRST REPAIR LOCATION THEN EVERY THIRD REPAIR LOCATION AFTERWARDS. WORK SHALL NOT PROCEED UNTIL THE CORE SAMPLE IS TAKEN AND ACCEPTED. ALL CORE SAMPLES AND HOLES SHALL BE INDEXED FOR FUTURE REFERENCE. THE ENGINEER SHALL DESIGNATE ALL LOCATIONS TO BE CORED. IF ANY CORES SHOW UNACCEPTABLE RESULTS, ALL WORK SHALL BE STOPPED UNTIL THE CONTRACTOR SUBMITS A PROPOSAL FOR CORRECTING UNACCEPTABLE WORK.

THE INITIAL CORE WILL ALSO SERVE TO QUALIFY THE FOREMAN FOR THIS WORK. IF AT ANY TIME A NEW FOREMAN IS USED, HE SHALL BE QUALIFIED WITH A CORE SAMPLE.

THE CONTRACTOR, AT HIS EXPENSE, SHALL REPAIR ALL CORE HOLES WITH AN APPROVED CEMENTITIOUS PATCHING MATERIAL.

CORE SAMPLES SHALL BE VISUALLY INSPECTED TO CONFIRM THAT CRACKS ARE COMPLETELY FILLED WITH ADHESIVE. ANY CORE HAVING LESS THAN 95% OF THEM CRACK FILLED WITH ADHESIVE SHALL BE CONSIDERED UNACCEPTABLE AND BE REJECTED.

CORE SAMPLES SHALL BE TESTED FOR BOND STRENGTH. SAMPLES MAY BE FRACTURED BY HAMMER BLOW TO CRACK AREA OR THROWN AT A HARD SURFACE. IF ADHESIVE FAILURE OCCURS BEFORE CONCRETE FAILURE, THE CORE SHALL BE CONSIDERED UNACCEPTABLE AND REJECTED.

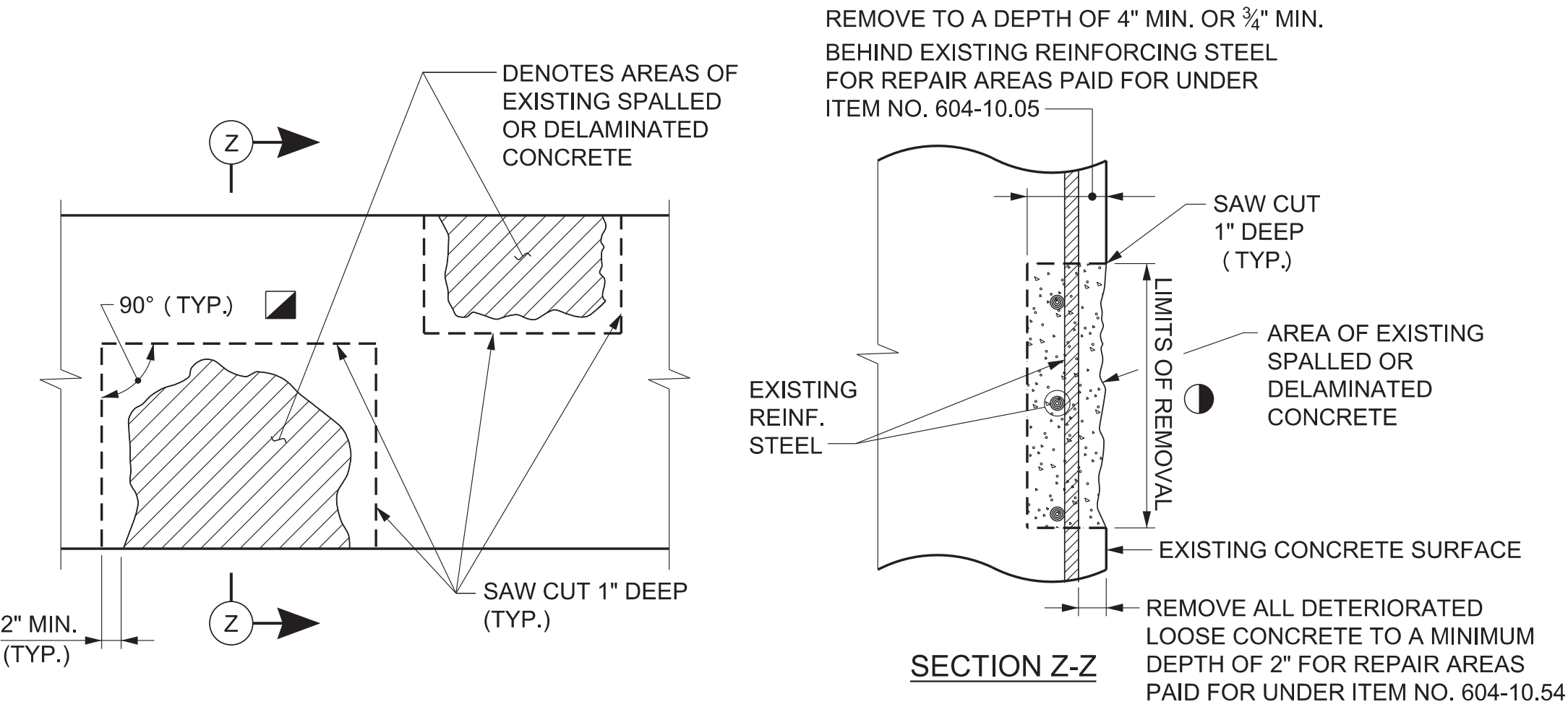
PAYMENT FOR EPOXY INJECTION CRACK REPAIR SHALL BE MADE UNDER ITEM NUMBERS 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. AND 604-10.58, EPOXY INJECTION (INJECTION), GAL.

PRICE BID FOR ITEM NUMBER 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. SHALL INCLUDE COST OF ALL LABOR AND MATERIALS (EXCEPT ADHESIVE) FOR GRINDING FOR SURFACE PREPARATION, CRACK PREPARATION, CAPPING, INJECTION OF ADHESIVE, ALL SAMPLES AND TESTING, REMOVAL OF CAPPING MATERIAL AND PORTS, AND OTHER INCIDENTALS. CRACKS SHALL BE MEASURED FOR PAYMENT ALONG THE LENGTH OF THE VISIBLE SURFACE CRACK.

PRICE BID FOR ITEM 604-10.58, EPOXY INJECTION (INJECTION), GAL., SHALL INCLUDE COST FOR ADHESIVE MATERIAL INJECTED ONLY.

NO PAYMENT SHALL BE MADE FOR REWORK DEEMED NECESSARY BY FAILURE OF ADHESIVE SAMPLES OR CORE SAMPLES.

ALL WORK INCLUDING SAMPLING AND TESTING SHALL BE IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE OR CONTRACT INSPECTORS. ANY WORK DONE WITHOUT INSPECTORS PRESENT SHALL NOT BE PAID FOR. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH WEEKLY SCHEDULES OF WORK TO BE PERFORMED. SCHEDULES SHALL BE SUBMITTED AT LEAST THREE (3) DAYS IN ADVANCE OF WORK TO BE DONE. THE ENGINEER SHALL BE NOTIFIED OF ANY CHANGE IN THE SCHEDULE A MINIMUM OF TWENTY -FOUR (24) HOURS IN ADVANCE OF CHANGE.



DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- - DENOTES LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER
- - DENOTES SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS

NOTES

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED OR DELAMINATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE AND INCORPORATED INTO THE NEW CONSTRUCTION. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALLED OR DELAMINATED AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13, SECTION B, MATERIAL QPL 13.009.

PNEUMATICALLY PLACED CONCRETE IS NOT ALLOWED.

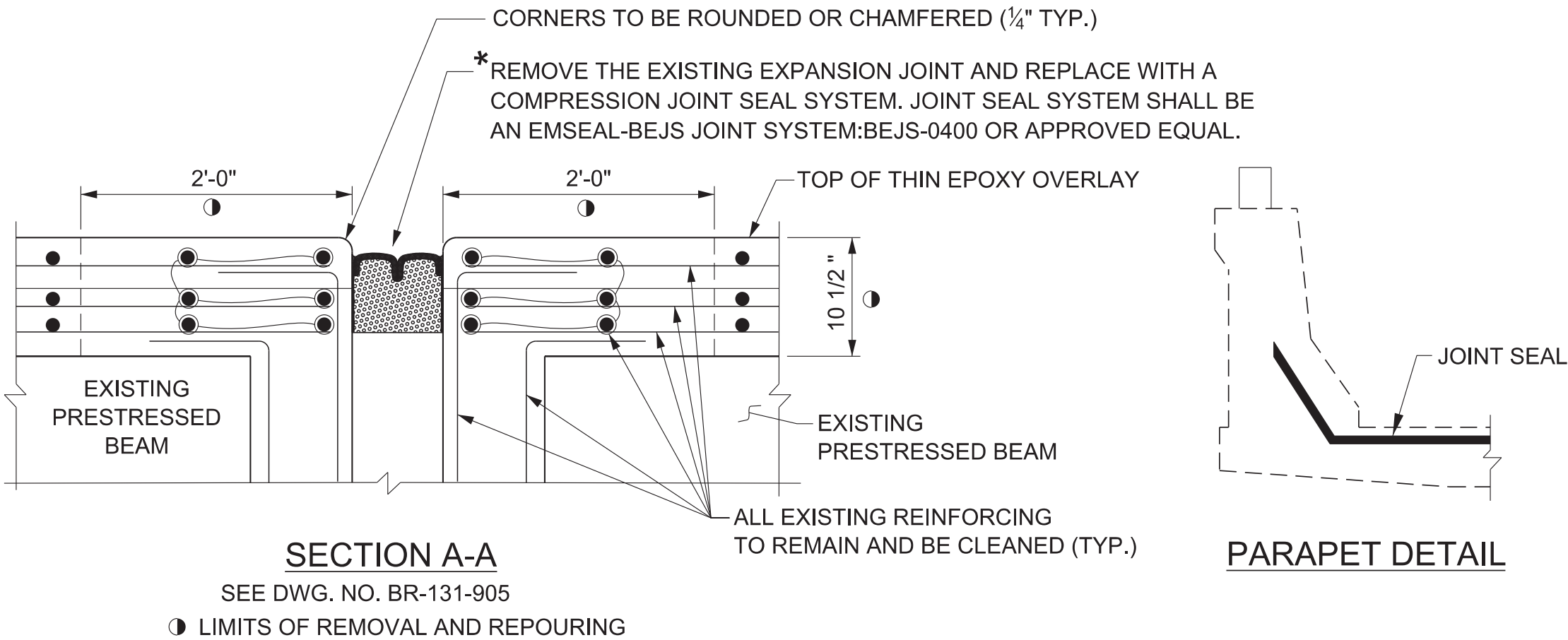
ANY REINFORCING IN THE REPAIR AREA SHOWING MORE THAN 20 PERCENT SECTION LOSS SHALL BE REPLACED.

COST OF SAW CUTTING, REMOVING SPALLED OR DELAMINATED CONCRETE, CLEANING, PATCHING MATERIAL, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. OR ITEM NO. 604-10.05, CONCRETE, S.F.

THE ENGINEER SHALL DESIGNATE ALL SPALLED OR DELAMINATED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND 604-10.54 MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER HAND DRIVEN TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

1. PNEUMATIC HAMMERS HEAVIER THAN THE 35 LB. CLASS SHALL NOT BE USED.
2. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL.



EXPANSION JOINT REPLACEMENT DETAILS

* THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE EXPANSION JOINT SUPPLIER FOR EACH JOINT IN THE BRIDGE. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION, INCLUDING GRINDING AND/OR GROOVING, PRIOR TO ANY JOINT MATERIAL INSTALLATION. THE JOINT SEAL MUST BE CONTINUOUS WITH NO FIELD SPLICES AND NO FIELD CUTTING.

COST OF REMOVAL OF THE EXISTING CONCRETE, PLATES, ANGLES, ANCHORS, CLEANING AND MAINTAINING THE EXISTING REINFORCING IN THE EXPANSION JOINT REPAIR AREAS, HIGH EARLY STRENGTH CONCRETE, FORMING, AND ALL MISCELLANEOUS MATERIALS AND LABOR NECESSARY TO INSTALL THE NEW COMPRESSION JOINT SEAL SYSTEM COMPLETE AND IN PLACE TO BE PAID FOR IN THE PRICE BID FOR ITEM NO. 604-10.44, EXPANSION JOINT REPAIRS.

TABLE OF JOINT WIDTHS

POURING TEMP.	EXPANSION JOINT WIDTH	
	BENT 1 LT	BENT 1 RT
40° F	2⅜"	2⅜"
50° F	2⅛"	2⅛"
60° F	2"	2"
70° F	1⅞"	1⅞"
80° F	1⅝"	1⅝"
90° F	1½"	1½"

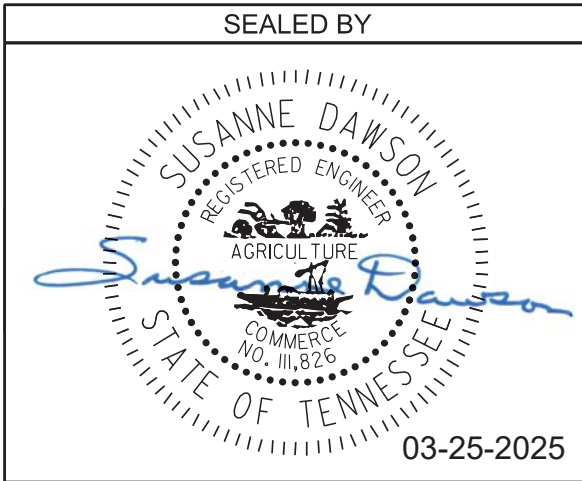
PROJECT NO.		YEAR	SHEET NO.
30S034-M3-002		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

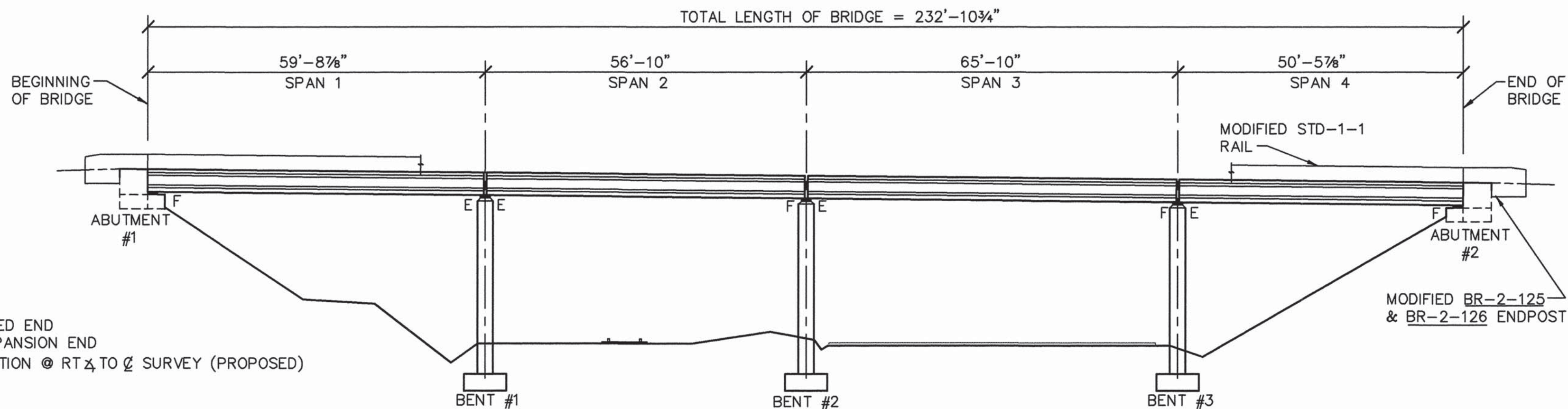
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS

BRIDGE NO. 30-SR34-17.25 (L & R)
FEDERAL BRIDGE ID NOS.
30FA0343003 & 30FA0343004
BRIDGES OVER SNAPPS FERRY RD.
& NFS RAILWAY, LM 17.25
GREENE COUNTY
2025

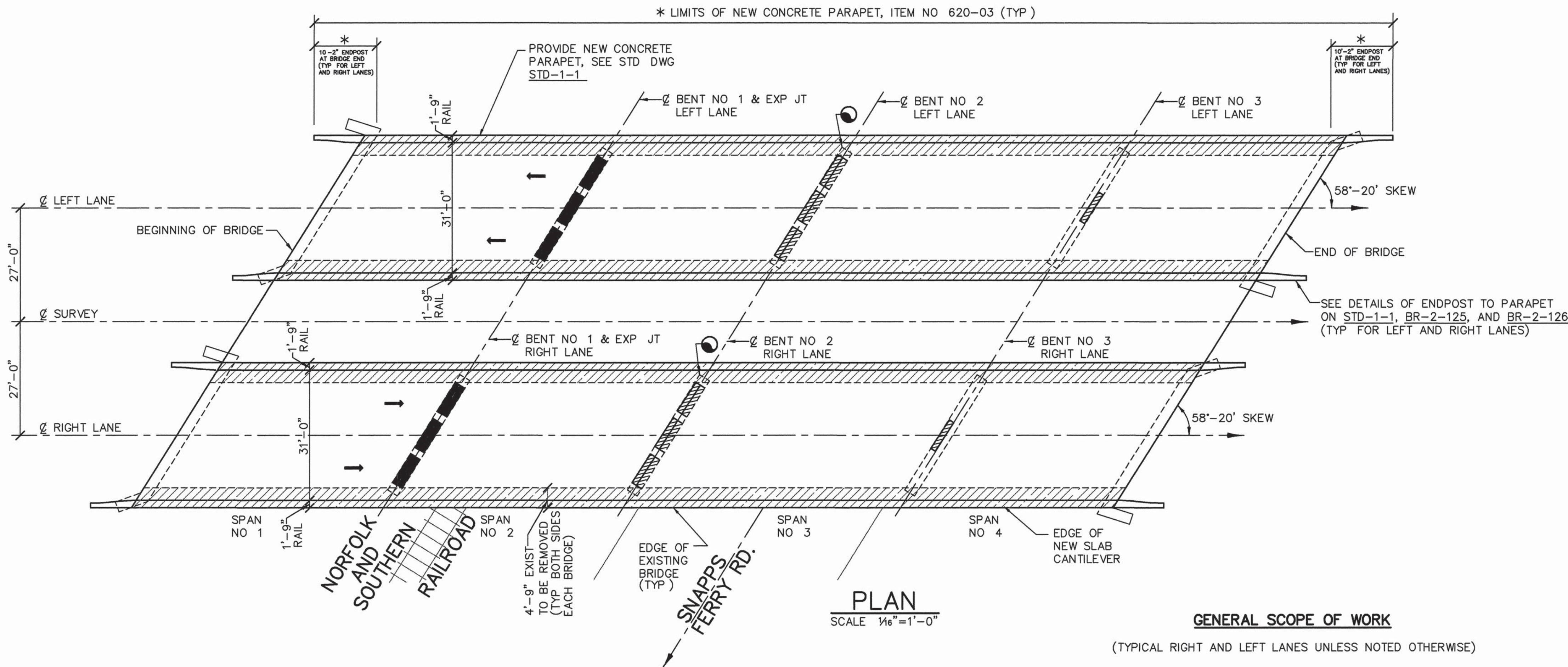
BR-131-915





- NOTES
- 1 F=FIXED END
E=EXPANSION END
 - 2 ELEVATION @ RT 3/4 TO 2 SURVEY (PROPOSED)

ELEVATION
SCALE 1/8"=1'-0"



PLAN
SCALE 1/8"=1'-0"

GENERAL SCOPE OF WORK

(TYPICAL RIGHT AND LEFT LANES UNLESS NOTED OTHERWISE)

- 1) SCARIFY ONE INCH (1") DEPTH FROM THE TOP OF EXISTING CONCRETE BRIDGE DECK FOR FULL LENGTH AND WIDTH OF THE BRIDGE, EXCLUDING THE AREA OF CANTILEVER SLAB TO BE REMOVED
- 2) REMOVE THE EXISTING BRIDGERAIL, GUARDRAIL, AND CONCRETE CANTILEVER
- 3) PERFORM FULL AND PARTIAL DECK REPAIRS ON THE EXISTING CONCRETE DECK
- 4) REPAIR CRACKS IN ENDWALL AT ABUTMENT NO 2, LEFT LANE MODIFY WINGWALLS AT ABUTMENT NO 1 AND NO 2 FOR ENDPOSTS
- 5) INSTALL NEW 4 1/2" CONCRETE OVERLAY WITH NEW REINFORCING STEEL IN PHASES
- * 6) INSTALL NEW JERSEY SHAPE CONCRETE PARAPET AND 10'-2" ENDPOSTS APPLY TEXTURE COATING TO THE PARAPET AND CANTILEVER
- 7) REPAIR DETERIORATED CONCRETE IN END CAPS AT BENT NO 2, NORTHS END ONLY
- 8) REPAIR CONCRETE CRACKS AND SPALLED AREAS AT DESIGNATED LOCATIONS
- 9) REPLACE DIAPHRAGMS AND INSTALL NEW TYPE "H" STRIP SEAL EXPANSION JOINT AT BENT NO 1
- 10) INSTALL EARTH BERM AT MEDIAN, BEGINNING AND END OF BRIDGE
- 11) TRANSITION ROADWAY APPROACH TO EACH END OF BRIDGE
- 12) REPAIR SOIL EROSION AT ABUTMENT NO 1 AND NO 2, LEFT LANE ONLY

REPAIR LEGEND

- LOCATIONS OF BENT CAP END REPAIR (2 LOCATIONS), SEE DETAILS AND NOTES ON DRAWING BR-16-79
- LOCATIONS OF DIAPHRAGMS TO BE REMOVED AND REPLACED SEE NOTES AND DETAILS ON DRAWING BR-16-81
- LOCATIONS OF CANTILEVER SLAB REPLACEMENT, SEE NOTES AND DETAILS ON DRAWINGS BR-16-76 AND BR-16-77
- LOCATIONS OF DIAPHRAGMS TO BE REPAIRED, SEE NOTES AND DETAILS ON DRAWING BR-16-78

INTERNATIONAL, LTD
CONSULTING ENGINEERS Nashville, Tennessee 37217
ENG JOB NO 94018

DESIGNED BY J ROBERTS DATE AUGUST 1994
DRAWN BY J WELLS DATE AUGUST 1994
SUPERVISED BY S RAMANNA, J MASON DATE AUGUST 1994
CHECKED BY J MASON, J ROBERTS DATE AUGUST 1994



PROJECT NO.		YEAR	SHEET NO.
30003-4230-04		1994	
REVISIONS			
NO	DATE	BY	BRIEF DESCRIPTION

LIST OF DRAWINGS

DRAWING NO	LAST REVISION DATE	DESCRIPTION
BR-16-74		LAYOUT OF BRIDGE TO BE REPAIRED
BR-16-75		ESTIMATED BRIDGE QUANTITIES AND GENERAL NOTES
BR-16-76		BRIDGE REPAIR DETAILS, 1 OF 6
BR-16-77		BRIDGE REPAIR DETAILS, 2 OF 6
BR-16-78		BRIDGE REPAIR DETAILS, 3 OF 6
BR-16-79		BRIDGE REPAIR DETAILS, 4 OF 6
BR-16-80		BRIDGE REPAIR DETAILS, 5 OF 6
BR-16-81		BRIDGE REPAIR DETAILS, 6 OF 6

LIST OF STANDARD DRAWINGS

DRAWING NO	LAST REVISION DATE	DESCRIPTION
BR-2-115	07-29-92	GENERAL NOTES AND DETAILS FOR EXPANSION JOINT REPLACEMENT CONSTRUCTION TYPES "A" THRU "J"-1991
BR-2-116	07-29-92	GENERAL NOTES AND DETAILS FOR STRIP SEAL EXPANSION JOINT REPLACEMENT CONSTRUCTION TYPES "A" THRU "J"-1991
BR-2-120	07-29-92	STRIP SEAL EXPANSION JOINTS REPLACEMENT CONSTRUCTION DETAILS TYPES "G" THRU "H"-1991
BR-2-125	11-16-92	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPOST-1988
BR-2-126	11-16-92	BRIDGE RAILING CONCRETE PARAPET-1990
STD-1-1	3-28-94	STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAWING DETAILS-1993
STD-1-2	5-19-94	

LIST OF REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)

DRAWING NO	LAST REVISION DATE	DESCRIPTION
K-27-108		LAYOUT OF BRIDGE
K-27-109		PRESTRESSED BEAM DETAILS, VARIABLE SECTIONS, STRAND LOCATIONS, AND QUANTITIES
K-15-65		STANDARD PRESTRESSED BEAM DETAILS
K-27-110		SUPERSTRUCTURE DETAILS
K-27-111		ABUTMENT DETAILS
K-27-112		BENT DETAILS
K-27-113		BILL OF STEEL
K-15-59		STANDARD CONCRETE HANDRAIL
H 5-11		PILES

LIST OF SPECIAL PROVISIONS

** DENOTES CURRENT REVISION DATE, AS PER CONTRACT DOCUMENTS

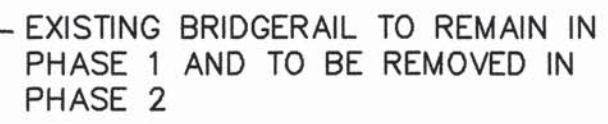
NO	LAST REVISION DATE	REGARDING
100	**	REVISIONS AND ADDITIONS TO STANDARD SPECIFICATIONS
105A	**	APPROVAL OF SHOP DRAWINGS
602	**	SECTION 602- STEEL STRUCTURES
603A	**	PAINTING
604	**	CONCRETE STRUCTURES
604CX	**	CONTRACTOR-MIX DESIGN AND TESTING STRUCTURAL CONCRETE
604R	**	RIDEABILITY OF BRIDGE DECKS AND ROADWAY APPROACHES
907A	**	EPOXY COATED REINFORCING STEEL



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

**LAYOUT OF BRIDGE
TO BE REPAIRED**
LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO. 30-SR34-17.25
GREENE COUNTY
1994

BR-16-74



WHEN REMOVING THE EXISTING CONCRETE
CANTILEVER SLAB, EXTREME CARE SHALL
BE TAKEN SO AS NOT TO DAMAGE THE
EXISTING TRANSVERSE REINFORCING STEEL

SPECIAL NOTE TO CONTRACTOR
(PERTAINING TO SCARIFYING BRIDGE DECK AND MAINTAINING TRAFFIC LANES) NO TRAFFIC SHALL BE PERMITTED AT ANY TIME ON THE SCARIFIED SURFACE OF THE BRIDGE DECK SEE SPECIAL PROVISION 604C THE CONTRACTOR SHALL BE REQUIRED TO SCARIFY IN TWO (2) PHASES IN ORDER TO COMPLY WITH THIS NOTE

SPECIAL NOTE

PARAPET DOWEL BARS (B471E) MUST BE IN PLACE BEFORE POURING
OVERLAY AND CANTILEVER SLAB

SEE CANTILEVER BOTTOM REINFORCING PLAN ON DRAWING BR-16-77



(250 MECHANICAL THREADED CONNECTORS REQ'D)-PER BRIDGE

SHOWING TRANSVERSE REINFORCING STEEL SPLICING
WITH MECHANICAL THREADED CONNECTORS

COST OF MECHANICAL THREADED CONNECTORS TO BE INCLUDED UNDER ITEM NO 604-10 18, REINFORCING STEEL (REPAIRS) LBS

CONNECTORS FOR SKEWED BARS SHALL BE INSTALLED AT 58°-28' ANGLE WITH APPROPRIATE FORM BLOCKOUTS. INSTALLATION SHALL MEET WITH THE FULL APPROVAL OF THE ENGINEER.



SCALE $\frac{3}{8}"=1'-0"$ (RIGHT LANE LOOKING FORWARD ON SURVEY)
(LEFT LANE LOOKING BACK ON SURVEY)



(SHOWING TOP MAT OF REINFORCEMENT ONLY)

SCALE $\frac{3}{32}'' = 1' - 0''$



BRIDGE REPAIR DETAILS
1 OF 6

LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO. 30-SR34-17.25
GREENE COUNTY
1994

BR-16-76



WHEN REMOVING THE EXISTING CONCRETE CANTILEVER SLAB, THE CONTRACTOR SHALL TAKE EXTREME CARE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL THAT IS DESIGNATED TO REMAIN THE EXISTING REINFORCING STEEL SHALL BE BLAST CLEANED AND INCORPORATED INTO THE NEW CONCRETE CANTILEVER

FOR DETAILS OF THE NEW CONCRETE PARAPET AND 10'-2" ENDPOST
SEE STANDARD DRAWING STD-1-1, BR-2-125, BR-2-126.

B471E

BARS A501E SPA @ 1'-0" FOR SPACING,
SEE PLAN VIEW ON DRAWING BR-16-76

2 1/2" CL

BARS B50_E OR A505E

3" CL

2" CL

BARS BA50_E
SPA @ 1'-0"

BARS B500E OR A505E
(4 SPA @ 1'-0")

4'-9"

EXISTING CONCRETE SLAB

EXISTING REINFORCING STEEL IN DECK

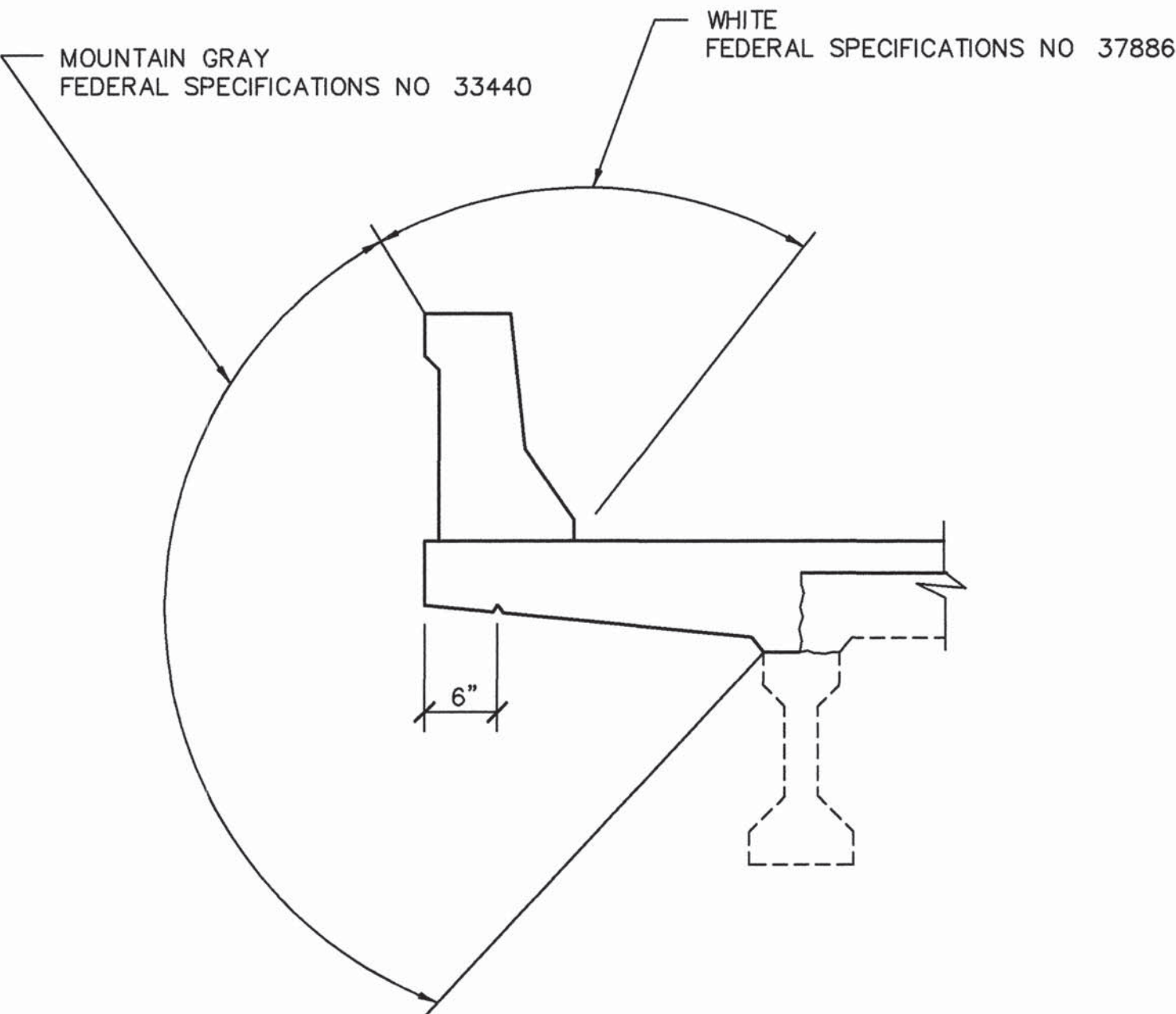
BEAM

BRIDGE & ROADWAY

DETAIL "A"

(SHOWING NEW CONCRETE PARAPET AND CANTILEVER REINFORCEMENT)

COST OF POURING THE NEW CONCRETE PARAPET, INCLUDING EPOXY COATED REINFORCING STEEL, FORMING, LABOR, AND ANY MISCELLANEOUS MATERIALS REQUIRED TO POUR THE PARAPETS, SHALL BE PAID FOR UNDER ITEM NO 620-03 CONCRETE PARAPET, LF



TEXTURE COATING DETAIL

CANTILEVER BOTTOM REINFORCING PLAN

DESIGNED BY J ROBERTS DATE AUGUST 1994
 DRAWN BY J WELLS DATE AUGUST 1994
 SUPERVISED BY S RAMANNA, J MASON DATE AUGUST 1994
 CHECKED BY J MASON, J ROBERTS DATE AUGUST 1994

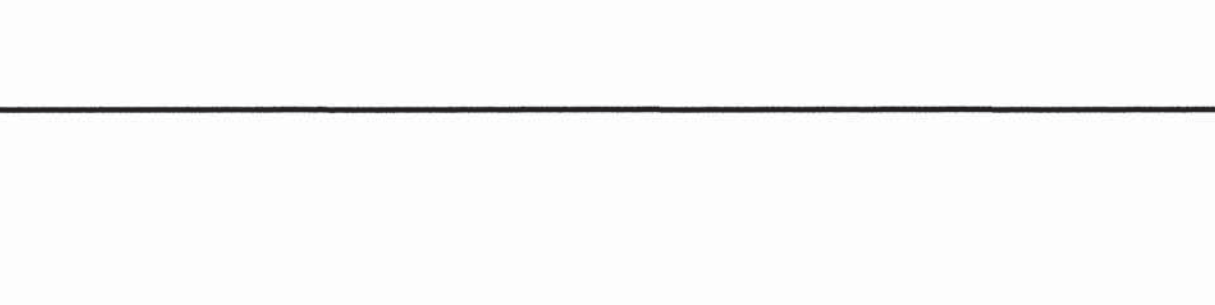


BRIDGE REPAIR DETAILS

2 OF 6

LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO. 30-SR34-17.25
GREENE COUNTY
1994

BR-16-77

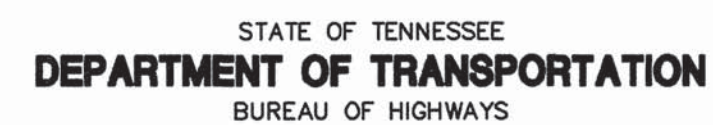


- 1 EXISTING SLAB AND DIAPHRAGM REINFORCING STEEL
TO REMAIN IN PLACE AND TO BE BLAST CLEANED
BEFORE POURING THE NEW CONCRETE
- 2 COST OF REMOVING EXISTING CONCRETE, FORMING
NEW CLASS "D" CONCRETE, AND ALL LABOR AND
OTHER MATERIAL NECESSARY FOR DIAPHRAGM
REPLACEMENT WILL BE INCLUDED IN ITEM NO
604-03 09, CLASS "D" CONCRETE (BRIDGE DECK) C.Y



1" SAW CUT (TYP)

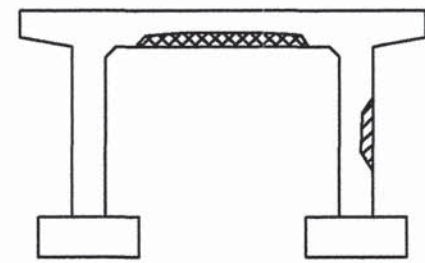
LIMITS OF DIAPHRAGM



LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO. 30-SR34-17.25
GREENE COUNTY
1994

BR-16-78

 REPAIR CONCRETE SPALLING ON BENT CAPS, SEE NOTES AND DETAILS
ON DRAWING BR-16-80



A schematic diagram of a bridge. It features two vertical piers supporting a horizontal deck. A truss structure, represented by a series of connected triangles, is shown on the deck between the piers. The bridge is supported by two rectangular foundations at the base of the piers.

A diagram of a bridge structure. It features a central arch supported by two piers. The arch is filled with a cross-hatch pattern. The piers are rectangular and also filled with a cross-hatch pattern. The bridge deck is shown as a horizontal line at the top, with a small section on the left side.

A schematic diagram of a beam with a central section removed. The beam is supported by two vertical columns. The removed section is indicated by a hatched pattern.

A schematic diagram of a beam with a central rectangular hole. A cross-hatched rectangular patch is shown on the top surface of the beam, centered above the hole. The patch is wider than the hole and extends to the edges of the beam's top flange.

* DIAPHRAGMS TO BE REPLACED

The diagram illustrates a concrete repair process. It shows a cross-section of a concrete slab with two areas of existing spalled concrete, indicated by diagonal hatching. These areas are enclosed by dashed rectangular lines. The repair is defined by saw cuts, shown as solid lines. Key dimensions and features include:

- SAW CUT 1" (TYP)**: Indicated for the top and bottom edges of the repair areas.
- 2" MIN TYP**: Indicated for the side edges of the repair areas.
- * 90 (TYP)**: Indicated for the corner of the repair area, showing a 90-degree angle.
- Denotes Area of Existing Spalled Concrete**: Two labels with arrows pointing to the hatched areas.
- A**: A reference point or axis, indicated by an arrow pointing right from a vertical line at the top and bottom of the diagram.

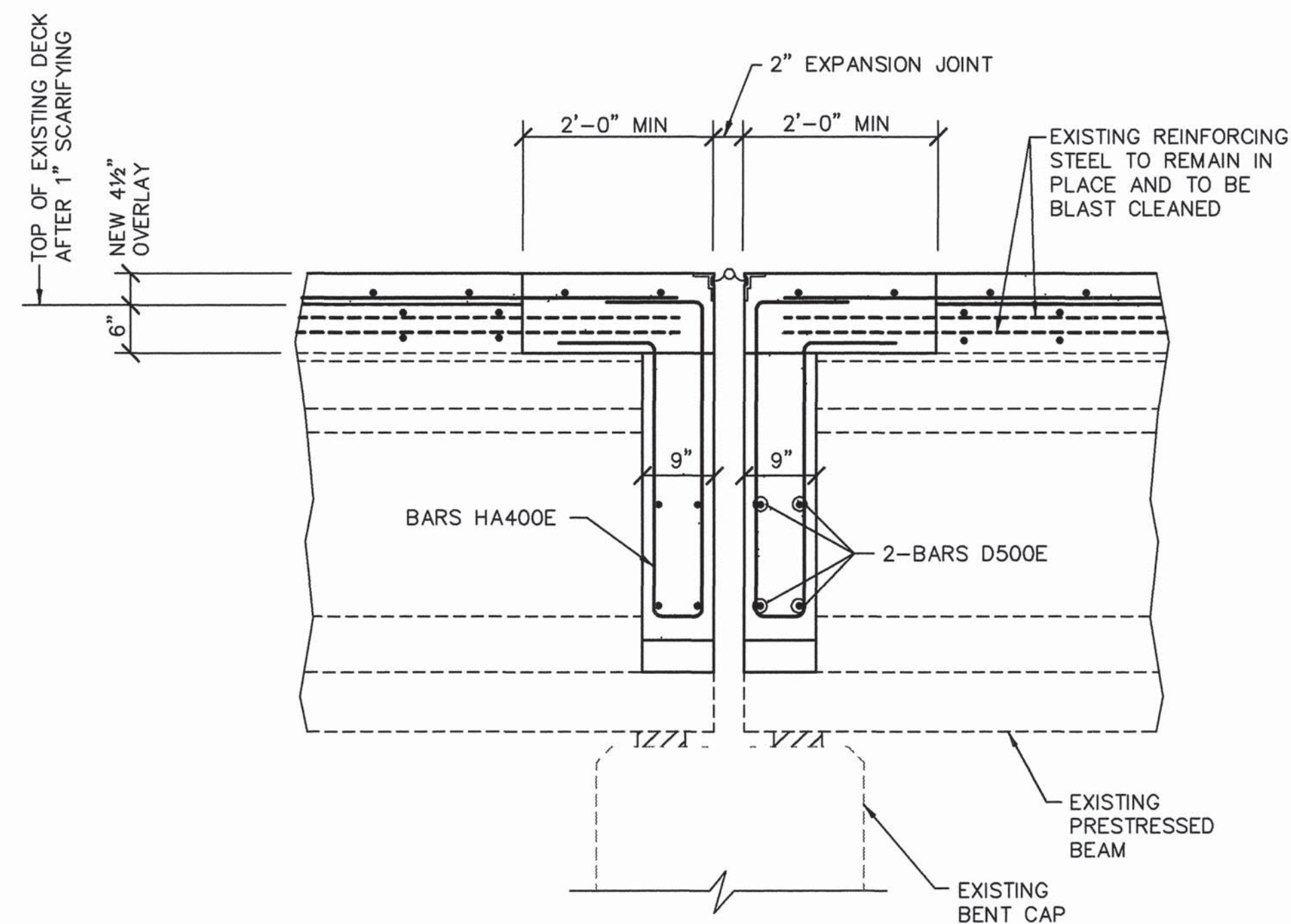
Diagram illustrating the repair procedure for a vertical concrete wall section:

- EXISTING CONCRETE SURFACE**: The top surface of the concrete to be repaired.
- REMOVE TO A DEPTH OF 4" MIN OR $\frac{3}{4}$ " MIN BEHIND THE EXISTING REINFORCING STEEL**: The depth of the repair area, measured from the existing surface.
- DENOTE SPALLED AREA**: The area of the concrete that has been removed or spalled.
- BLAST CLEAN EXISTING REINFORCING STEEL**: The process of cleaning the existing reinforcing steel.
- 2" MIN (TYP)**: The typical thickness of the new concrete repair.
- LIMITS OF REMOVAL**: The vertical extent of the repair area.
- FORM NEW CONCRETE SURFACE IN LINE WITH ADJACENT CONCRETE SURFACE**: The final step, ensuring the new concrete surface is flush with the existing structure.
- REMOVE TO A DEPTH OF $\frac{3}{4}$ " MIN BEHIND EXISTING REINFORCING STEEL**: The depth of the repair area, measured from the existing surface.

Diagram illustrating a repair area on an existing concrete surface. The repair area is defined by dimensions: a width of (4" ±) and a height of 3" MIN. The repair area is labeled "DENOTES EXISTING CRACKS AND SPALLED CONCRETE". A saw cut is indicated as "SAW CUT 1" (TYP)". The existing concrete surface is labeled "EXISTING CONCRETE SURFACE".

DESIGNED BY J ROBERTS DATE AUGUST 1994
 DRAWN BY J WELLS DATE AUGUST 1994
 SUPERVISED BY S RAMANNA, J MASON DATE AUGUST 1994
 CHECKED BY J MASON, J ROBERTS DATE AUGUST 1994

LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO 30-SR34-17.25
GREENE COUNTY
1994



DETAIL "B"

SHOWING AREAS OF REPAIR FOR TYPE "H"
EXPANSION JOINT REPLACEMENT AND DIAPHRAGMS
AT BENT NO 1 (LEFT AND RIGHT LANES)

NOTES:

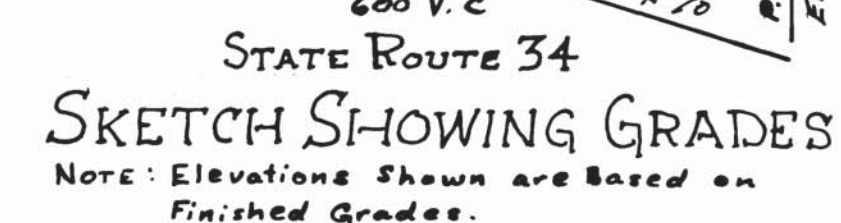
COST OF ALL FORMING, SAW CUTTING, WELDING, GROUT,
CLEANING, LABOR, AND ANY MISCELLANEOUS MATERIALS
NEEDED TO COMPLETE THE REPAIRS AS SHOWN SHALL
BE INCLUDED IN THE ITEMS BID ON

COST OF REMOVING EXISTING CONCRETE, FORMING NEW CLASS "D" CONCRETE, AND ALL LABOR AND OTHER MATERIAL NECESSARY FOR DIAPHRAGM REPLACEMENT, WILL BE INCLUDED IN ITEM NO 604-03 09, CLASS "D" CONCRETE, (BRIDGE DECK) C Y



LEFT AND RIGHT LANES
STATE ROUTE 34
OVER
NORFOLK-SOUTHERN RAILROAD
AND SNAPPS FERRY ROAD
BRIDGE NO 30-SR34-17.25
GREENE COUNTY
1994

BR-16-81



* MEASURED AT RT. 4ST TO C.R.R.

SPECIFICATIONS: Standard Road & Bridge Specifications of the
Tennessee Department of Highways.

LOADING: H20-516-44

CONCRETE: Cast in Place to be Class "A"

REINFORCING STEEL: To be Intermediate or Hard Grade. Standard

hook details as recommended by C.R.S.I. shall apply

DESIGN : See 1961 A.A.S.H.O. Specifications.

NEOPRENE BEARING PAD: See Special Provisions

FILES: 10" B.P. @ 42"

Foundations for Bents shall be excavated to Pile cut-off elevations shown; red Soundings shall be made as directed by the Engineer. From the results obtained the Engineer will decide if Piles will be used, or the footings carried to rock. If rock is used holes 6' deep shall be drilled into the rock at points designated by the Engineer. If piles are used the Piles shall be driven to rock or minimum bearing 36 tons. No reinforcing steel for columns shall be ordered until final testing shows an established. Cost of red sounding shall be included in the cost of items bid on.

Build handrail according to Standard Drawing K-15-59 except
Use endpost as shown on Drawing K-27-111. Dimension X = $1\frac{1}{2}$;
L = $6'-4\frac{1}{2}$ (29 spaces); Dimension Q = $229\frac{1}{2}$ "

DWG. No.

HANDRAIL: See Note This Sheet

PRESTRESSED BEAM DETAILS _____ K-15-59

SUPERSTRUCTURE _____

ABUTMENTS NO. 1 & 2 EACH LANE

BENTS No. 1, 2 & 3 EACH LANE.

BILL OF STEEL _____

PILES _____

2-28'-0" ROADWAYS WITH SAFETY CURBS

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE

LAYOUT OF BRIDGE
LEFT AND RIGHT LANES
STATE ROUTE 34 OVER SOUTHERN
RAILROAD AND SNAPP FERRY ROAD
STATION 202+50.76
GREENE COUNTY
7-1963-

7. 10 -1963-

CORRECT Fred Greve

BRIDGE ENGINEER

K-27-108



ITEM	Excavation Cu. Yds.		Concrete Class ¹ Cu. Yds.	Reinforcing Steel Lbs.	Prestressed Concrete Bents				Concrete Vandrail lin. Ft.	Rock Drilling lin. Ft.	Steel 10" Bore lin.
	Dry	Rock			58'3	56'9	65'9	49'0			
Superstructure			217.5	49,179							
Abutment No.1			19.8	1,603							
Bent No.1	32		35.5	5,700							
Bent No.2	42		34.4	5,552							
Bent No.3	37		34.2	5,512							
Abutment No.2			19.8	1,603							
Totals	111	20	361.2	69,149	4	4	4	4	460	36	110
Superstructure			217.5	49,179							
Abutment No.1			19.9	1,603							
Bent No.1	32		35.6	5,714							
Bent No.2	42		34.6	5,579							
Bent No.3	37		34.3	5,552							
Abutment No.2			19.9	1,603							
Totals	111	20	361.8	69,230	4	4	4	4	460	36	110
Total Both Lanes	222	40	723.0	138,379	8	8	8	8	920	72	220



DESIGNED BY R. Green DATE 3-19-63
DRAWN BY W. D. Brinton Jr. DATE 4-1-63
TRACED BY _____ DATE _____
CHECKED BY R. Green ; A. McCord DATE 5-1-63

Revised 6/5/63 Foundation Note.

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
3	TENN.		19	12	88

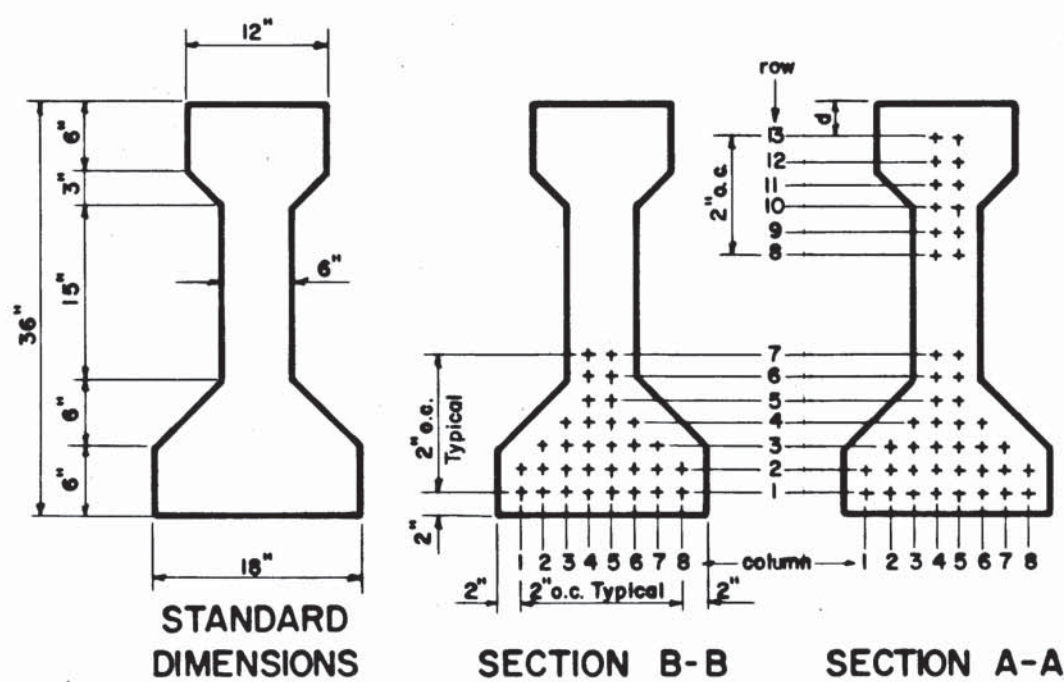
STANDARD DIMENSIONS

SECTION B-B

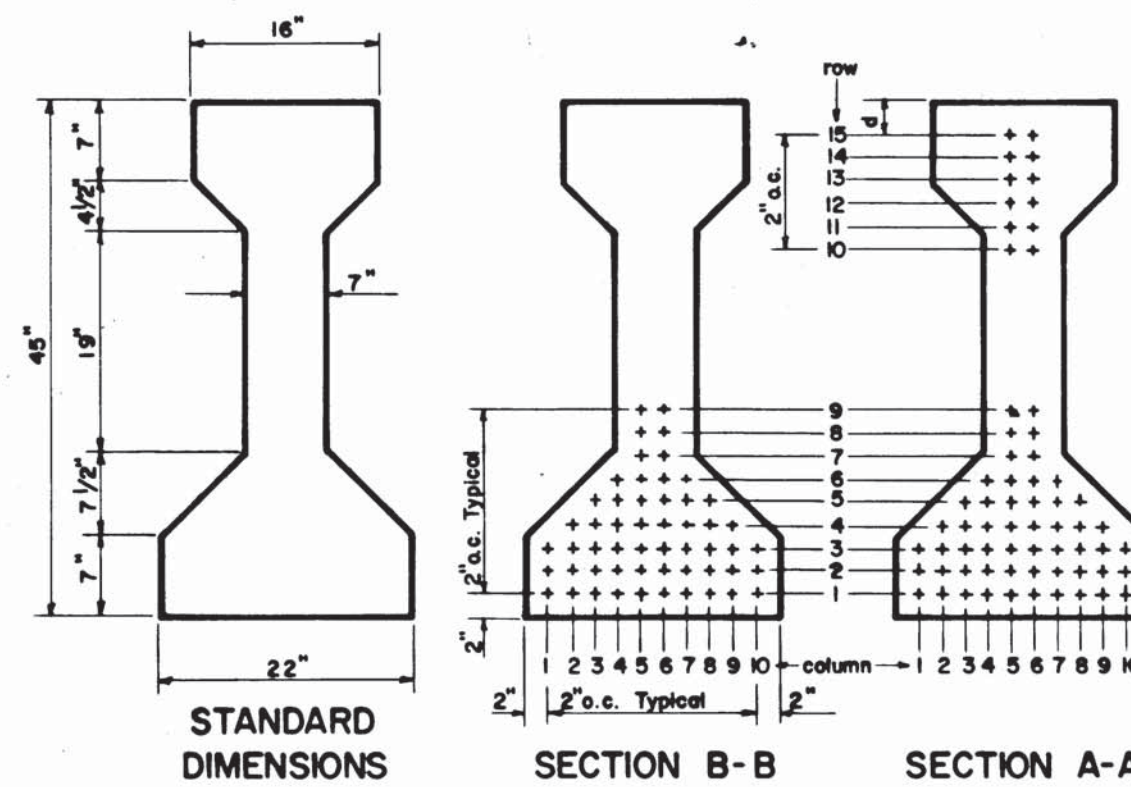
SECTION A-A

TYPE I

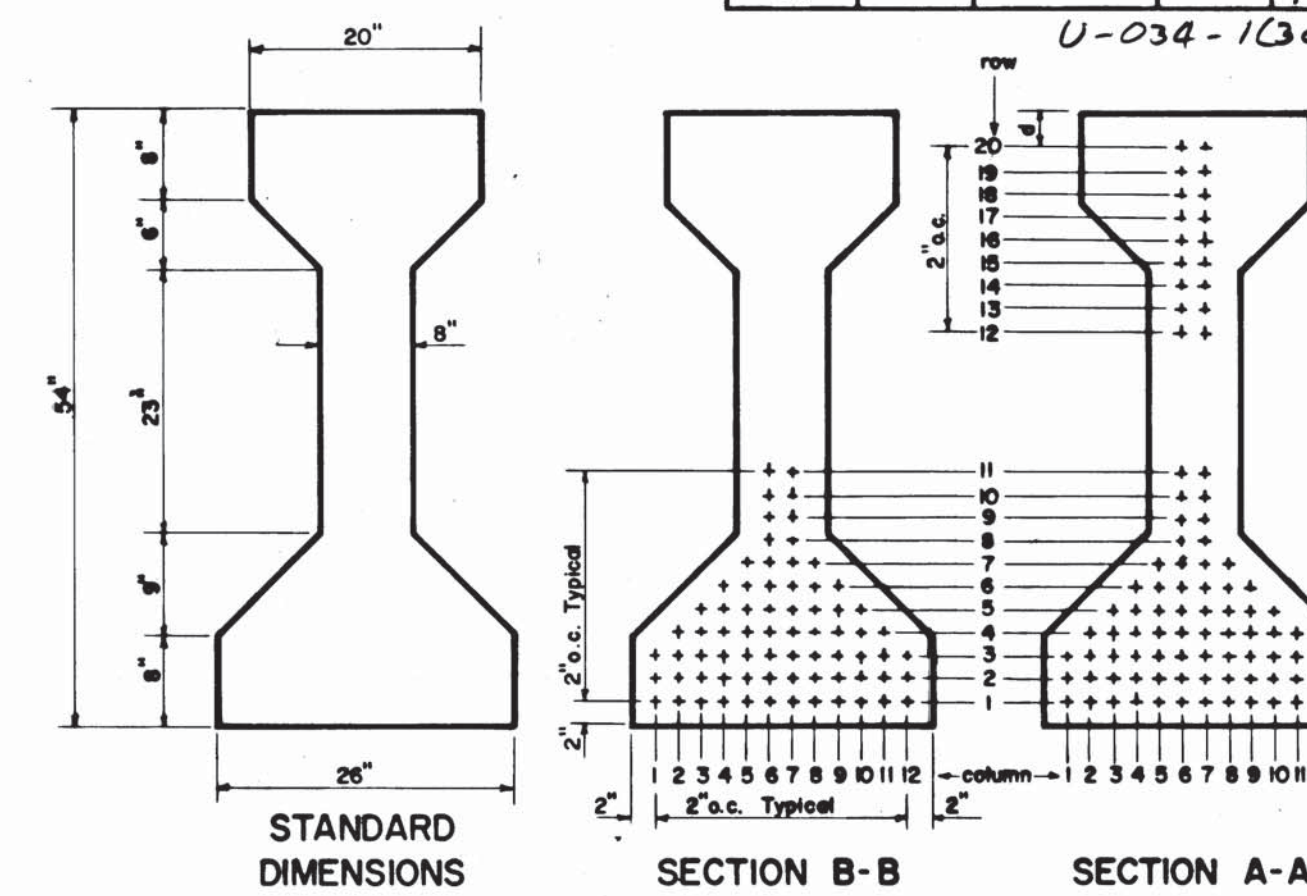
* TOTAL FOR BOTH LANES



TYPE 2



TYPE 3



TYPE 4

[illegible]

BRIDGE

STATE ROUTE 34/
SOUTHERN R.R. AND
SNAPP FERRY RD.

STRAND LOCATIONS

Size
7/16" ϕ

			SECTION B-B												SECTION A-A														
BEAM MARKING	TYPE	*NO. REQ'D	Location row	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11	12	Location row	
				I3F1	3	2	1																						
I3I1	3	2	2																								2		
			3																								3		
			4																								4		
			5																								5		
I3I2	3	2	6																								6		
			7																								7		
I3F2	3	2	8																								8		
Letter Dimensions				9																									9
L = 56'-9" T = 5'-9"				10																									10
X = 5" d = 8 1/2"				11																									11
Stirrup Spacing = 1'-7"				12																									12
Bar	*No. Req'd	Length		13																									13
				14																									14
H400	720	3'-4"		15																									15
				16																									16
H500	80	3'-4"		17																									17
				18																									18
				19																									19
A500	32	28'-9"		20																									20
Strand Totals				32	3	3	3	3	4	4	3	3	3	3	3	3	3	3	4	4	3	3	3	3	32				

BRIDGE

STATE ROUTE 34 / SOUTHERN R.R. AND SNAPP FERRY RD.

STRAND LOCATIONS

Size 7/16"

SECTION B-B

SECTION A-A

BEAM MARKING	TYPE	*NO. REQ'D	Column	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11	12	Row
I3F3	3	2	row																								1	
			2																								2	
			3																								3	
I3I3	3	2	4																								4	
			5																								5	
I3I4	3	2	6																								6	
			7																								7	
I3F4	3	2	8																								8	
			9																								9	
Letter Dimensions			10																								10	
L = 65'-6" T = 6'-6"			11																								11	
X = 5" d = 7 1/8"			12																								12	
Stirrup Spacing = 1'-4"			13																								13	
Bar	*No. Req'd	Length	14																								14	
H400	928	5'-4"	15																								15	
H500	80	3'-4"	16																								16	
			17																								17	
			18																								18	
			19																								19	
A500	32	33'-3"	20																								20	
Strand Totals			42	3	4	4	5	5	5	5	4	4	3	3	4	4	5	5	5	4	4	3				21		

BRIDGE			STRAND LOCATIONS												Size 7/16"
STATE ROUTE 34/ SOUTHERN R.R. AND SNAPP FERRY RD.			SECTION B-B												
BEAM MARKING	TYPE	*No. Req'd	Strand row	1	2	3	4	5	6	7	8	9	10	11	12
E3F3	3	2	1	X	X	X	X	X	X	X	X	X	X	X	X
E3I3	3	2	2												
E3I4	3	2	3												
E3F4	3	2	4												
Letter Dimensions			5												
L = 49'-0" T = 4'-4"			6												
X = 5" d = 7'-5"			7												
Stirrups Spacing = 14'-0"			8												
Bar	*No. Req'd	Length	9												
			10												
H400	576	5'-4"	11												
H500	80	3'-4"	12												
A500	32	24'-0"	13												
Strand Totals			14												
			15												
			16												
			17												
			18												
			19												
			20												
			21	2	2	2	3	3	3	3	2	2	2	2	2
			22	2	2	2	3	3	3	3	2	2	2	2	2

[illegible][illegible][illegible][illegible][illegible]

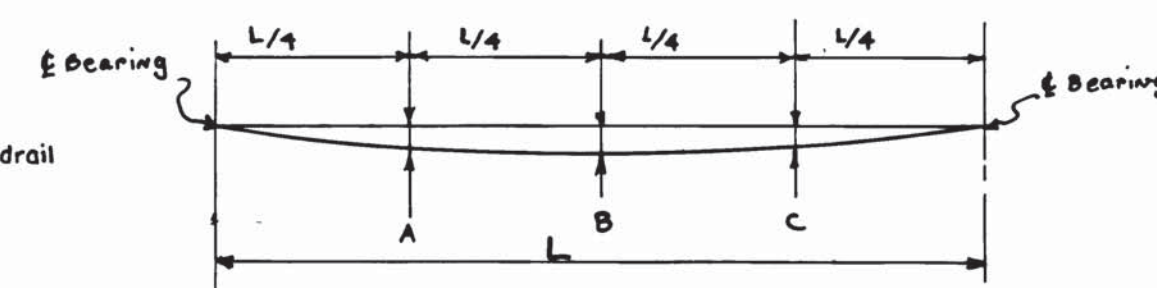
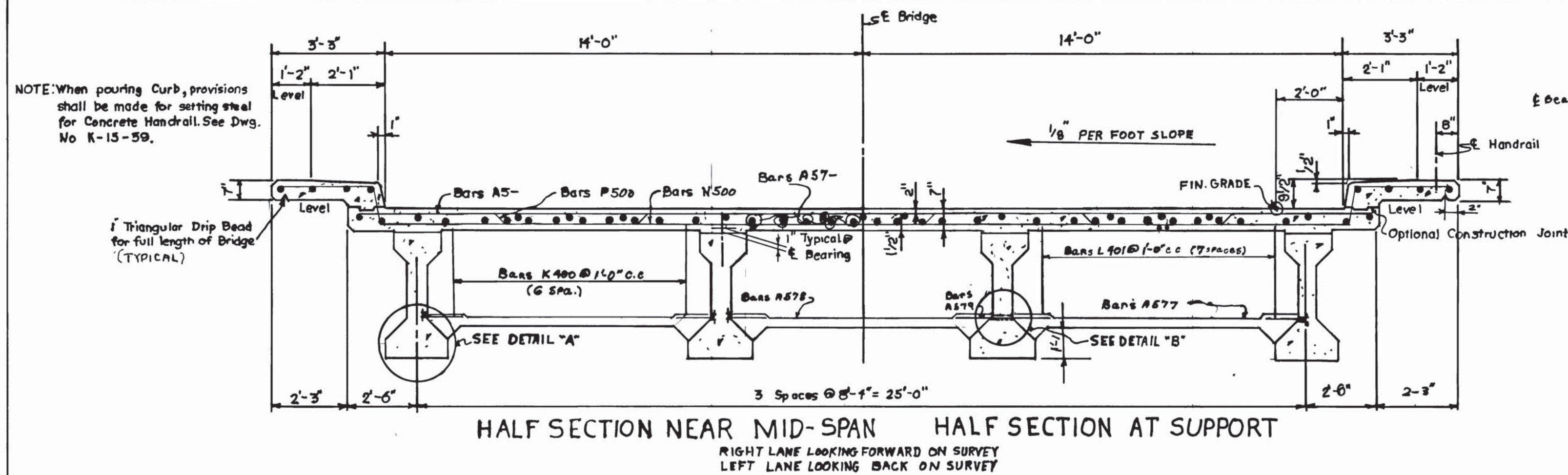
ESTIMATED QUANTITIES PER BEAM

DESIGNED BY	R. GREEN	DATE	4-19-63
DRAWN BY	Joe Parkes	DATE	
TRACED BY	R. Hagar & J. WYNN	DATE	4-26-63
CHECKED BY	R. Green, A. McCord	DATE	5/1/63

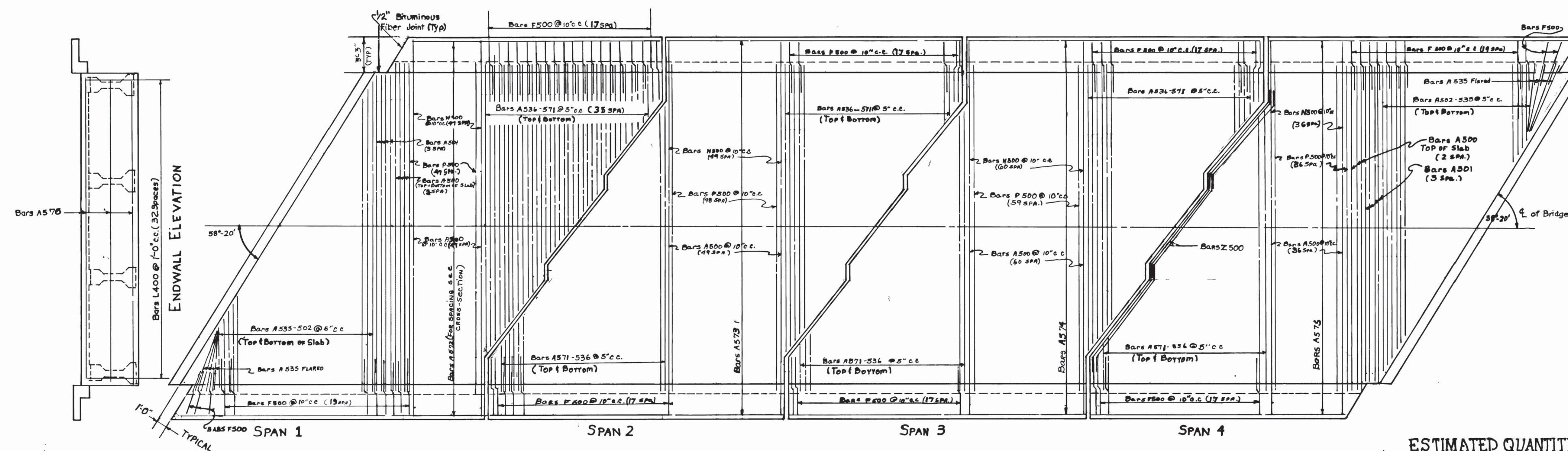
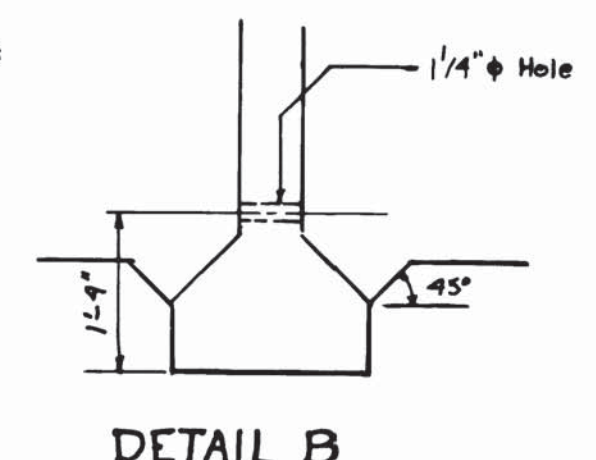
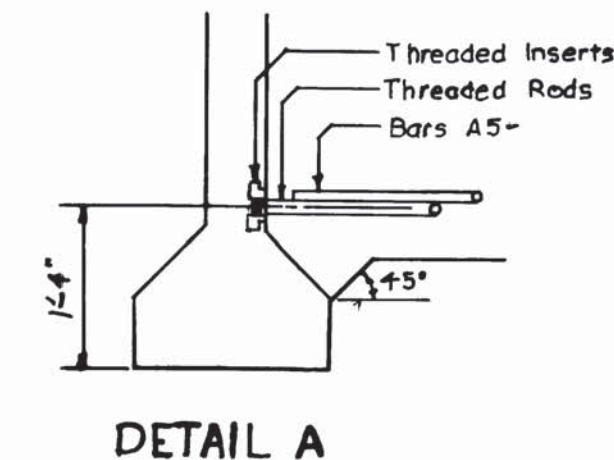
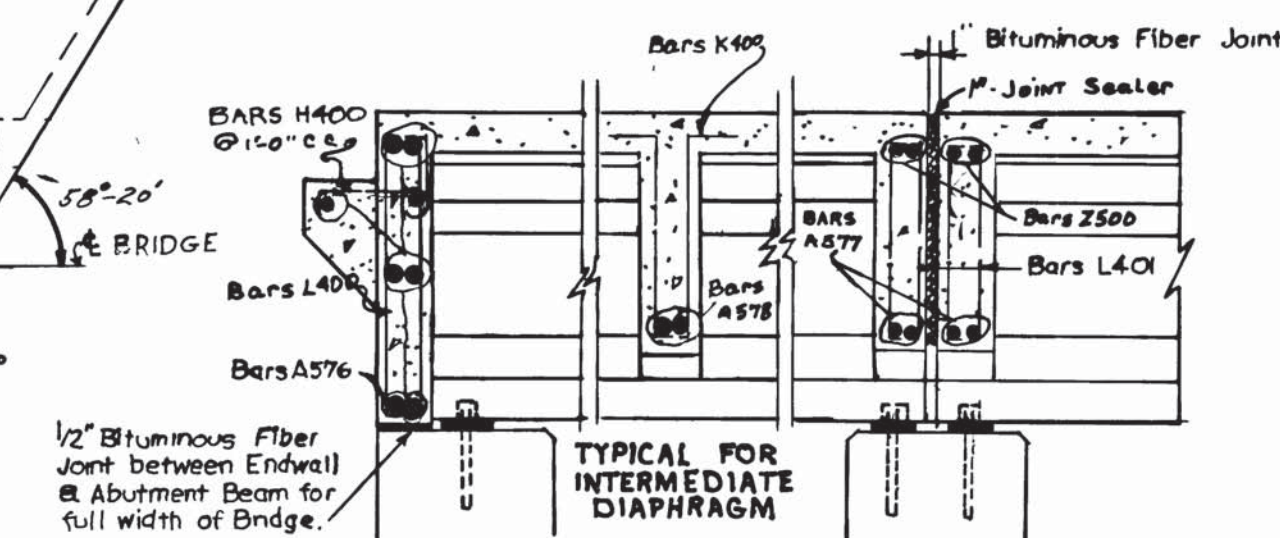
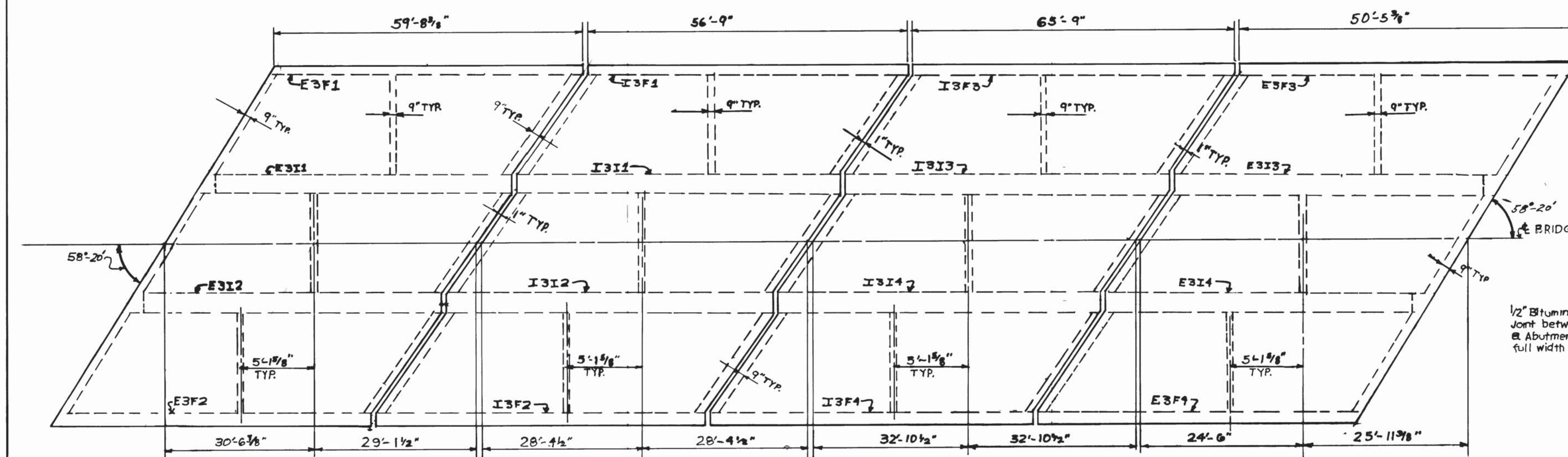
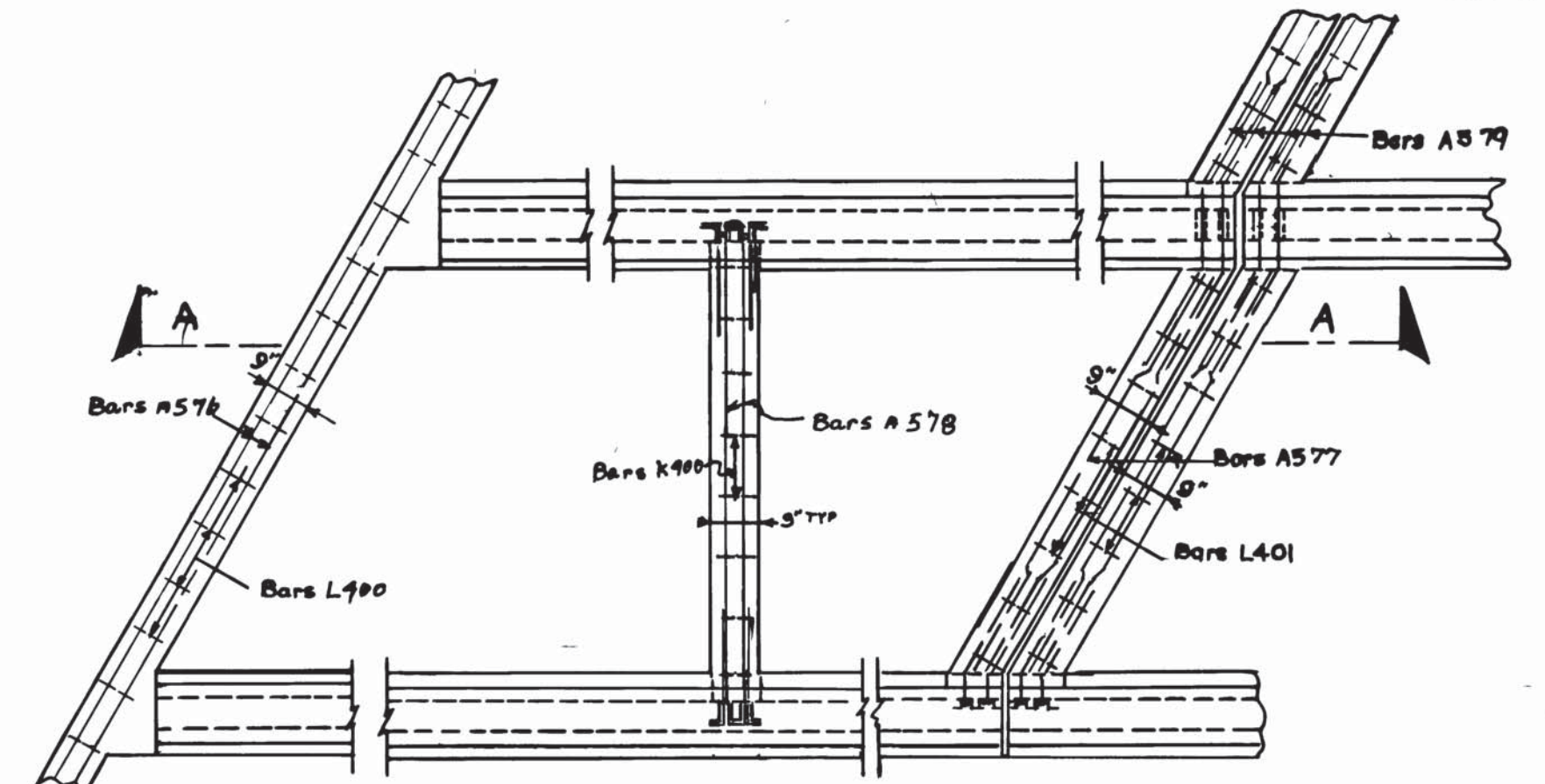
STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE
VARIABLE PRESTRESSED BEAM DETAILS
SECTIONS, STRAND LOCATIONS, & QUANTITIES
LEFT AND RIGHT LANES
STATE ROUTE 34 OVER SOUTHERN R.R.
AND SNAPP FERRY ROAD
STATION 202+50.76
GREENE COUNTY
•1963•

CORRECT Fred Goore
BRIDGE ENGINEER
APPROVED Will Long
STATE HIGHWAY ENGINEER

K-27-109



L	DEFLECTIONS		
	A	B	C
57.25	1/4"	3/8"	1/4"
55.75	1/4"	3/8"	1/4"
64.75	3/8"	5/8"	3/8"
48.00	1/8"	1/4"	1/8"



DESIGNED BY R. GREEN DATE _____
 DRAWN BY J. WYNN DATE _____
 TRACED BY _____ DATE _____
 CHECKED BY _____ DATE _____

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE

**SUPERSTRUCTURE DETAILS
LEFT AND RIGHT LANES
STATE ROUTE 34 OVER SOUTHERN R.R.
AND SNAPP FERRY ROAD
STATION 202+50.76
GREEN COUNTY
1963**

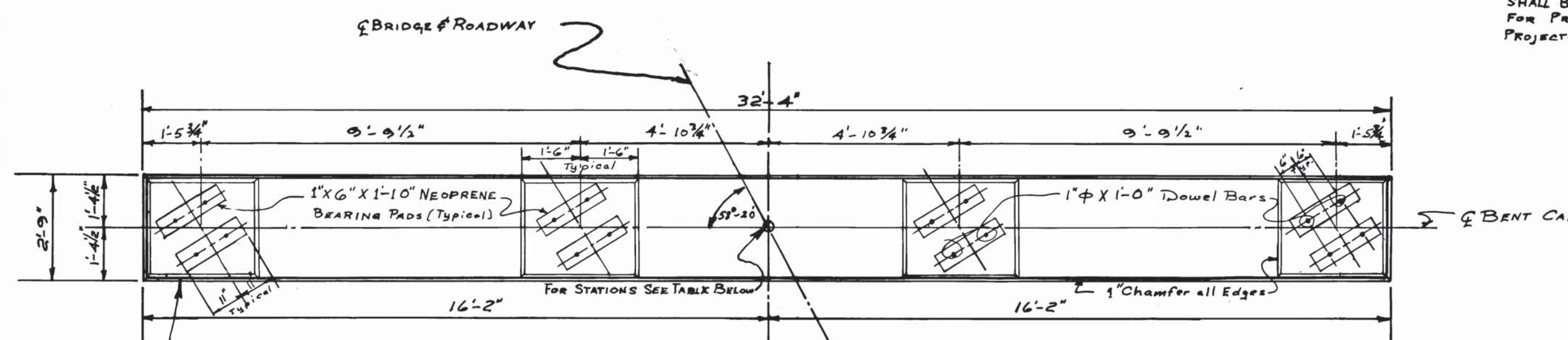
ESTIMATED QUANTITIES		
ITEM	CONCRETE CLASS "A"	REINFORCING STEEL LBS.
LEFT LANE	217.5	49,179
RIGHT LANE	217.5	49,179

CORRECT Fred Grove
APPROVED Lee DeLong

-27-110

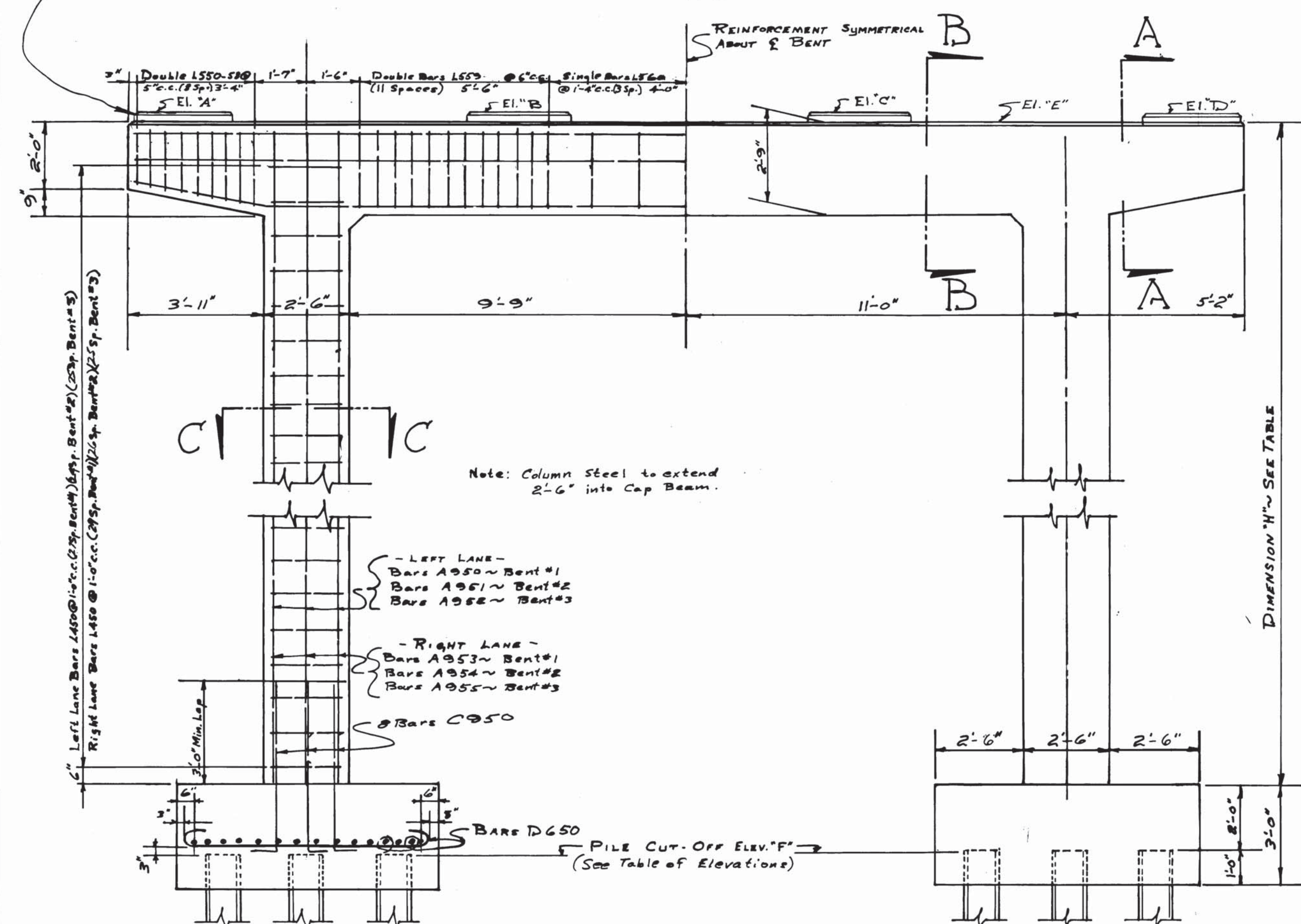
U-034-1(30)

NOTE: WHEN POURING CAP BEAM PROVISIONS SHALL BE MADE FOR SETTING DOWEL BARS FOR PRESTRESSED BEAMS. DOWEL BAR PROJECTION 3/4"



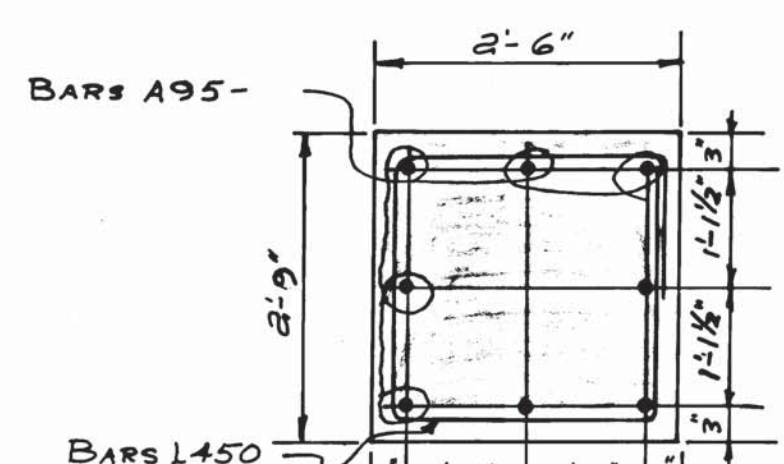
- PLAN -

Note: Leave this block off on left lane only.

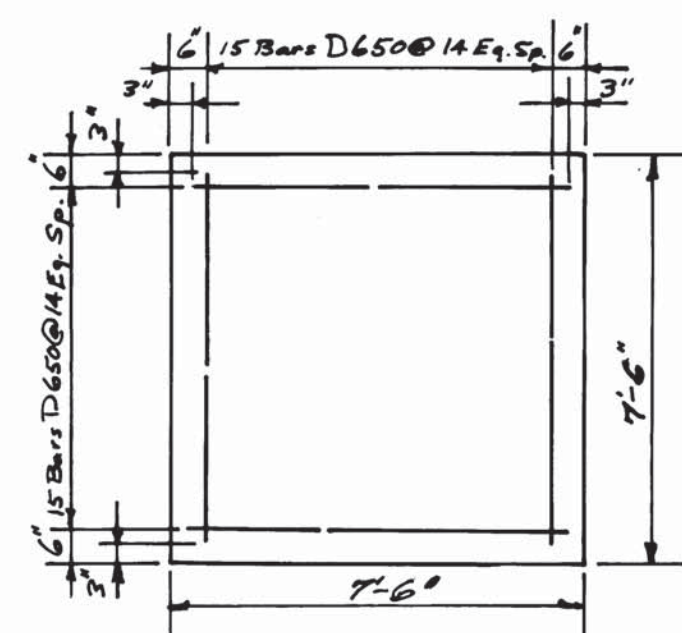


ELEVATION BENTS No. 1, 2 & 3

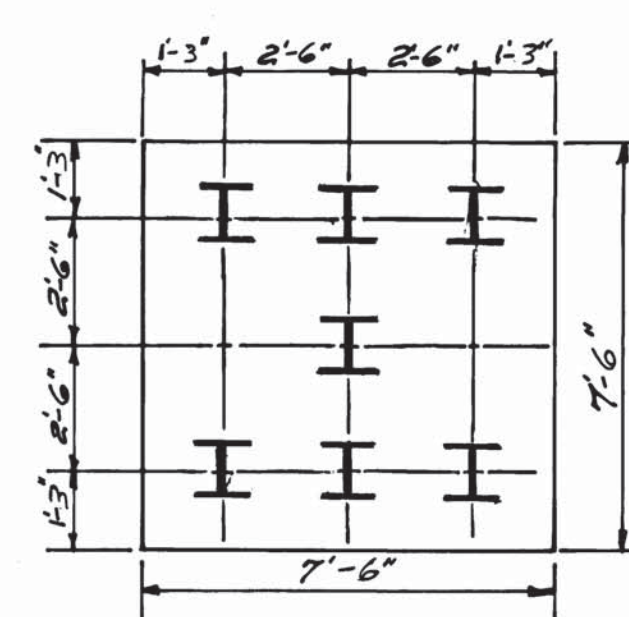
NOTE: LEFT LANE LOOKING FORWARD ON SURVEY.
RIGHT LANE LOOKING BACK ON SURVEY.



- SECTION C-C -

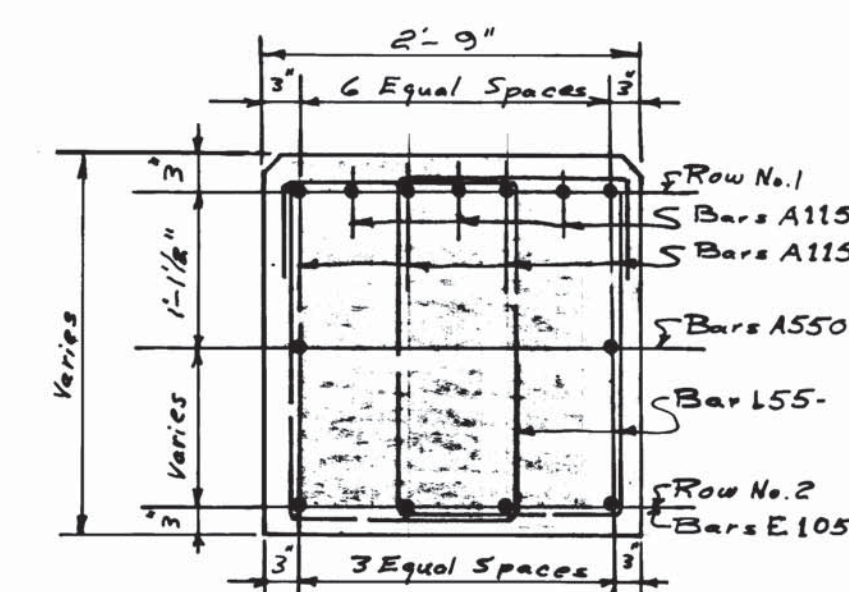


PLAN OF REINFORCEMENT

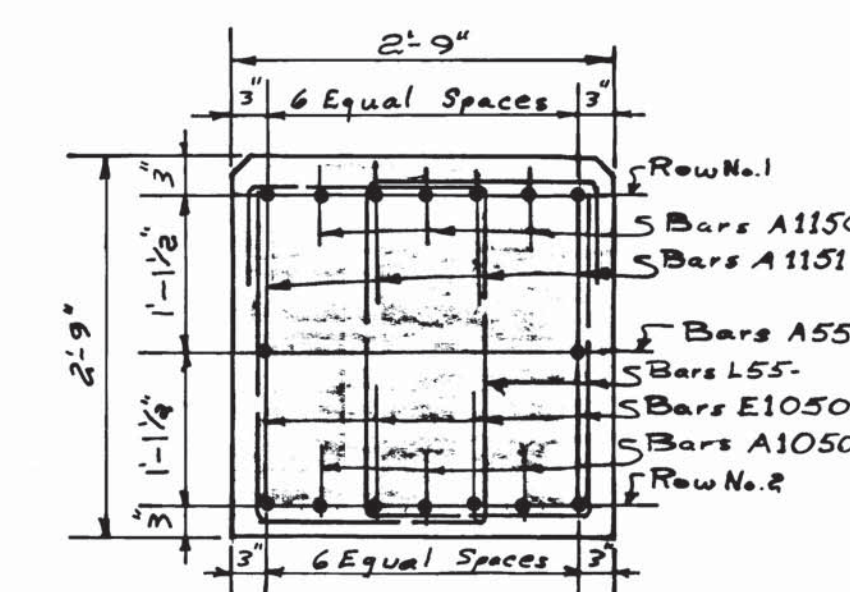


SECTION D-D OF PILE ARRANGEMENT

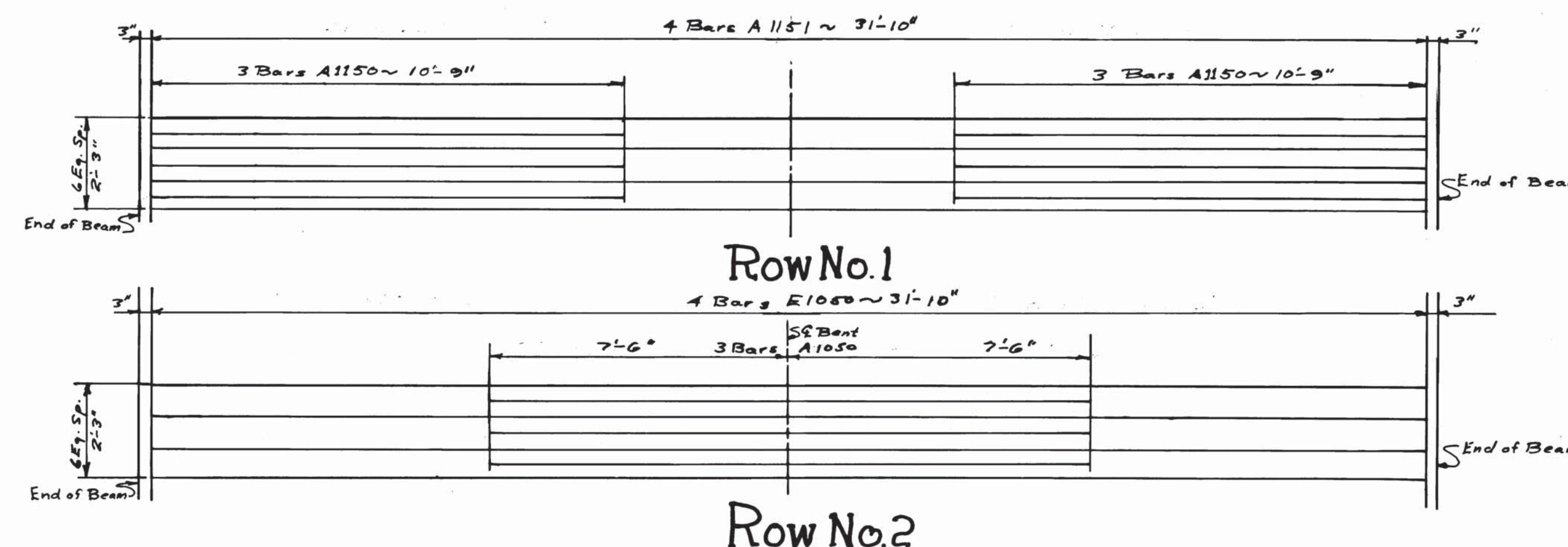
- PLAN OF FOOTING -



- SECTION A-A -



- SECTION B-B -



PLAN OF MAIN REINFORCEMENT ~ CAP BEAM

TABLE OF STATIONS, ELEVATIONS & DIMENSIONS

ITEM	STATIONS	ELEVATIONS						DIM. "H"
		"A"	"B"	"C"	"D"	"E"	"F"	
Bent #1	203+27.15	1581.65	1581.80	1581.95	1582.11	1581.65	1549.50	30'-3"
Bent #2	203+83.98	1580.84	1581.00	1581.16	1581.33	1580.84	1550.83	28'-0"
Bent #3	204+49.81	1579.78	1579.95	1580.12	1580.30	1579.78	1550.35	27'-6"
Bent #1	202+93.85	1582.26	1582.43	1582.60	1582.77	1582.26	1549.50	30'-6"
Bent #2	202+50.68	1581.54	1581.71	1581.88	1582.05	1581.54	1550.83	28'-6"
Bent #3	204+16.51	1580.68	1580.85	1581.02	1581.19	1580.68	1550.35	28'-0"

- ESTIMATED QUANTITIES -

ITEM	CONCRETE CLASS "A" Cu. Yds.	REINFORCING STEEL Lbs.
BENT No. 1	35.5	5,700
BENT No. 2	34.4	5,552
BENT No. 3	34.2	5,512
BENT No. 1	35.6	5,714
BENT No. 2	34.6	5,579
BENT No. 3	34.3	5,552

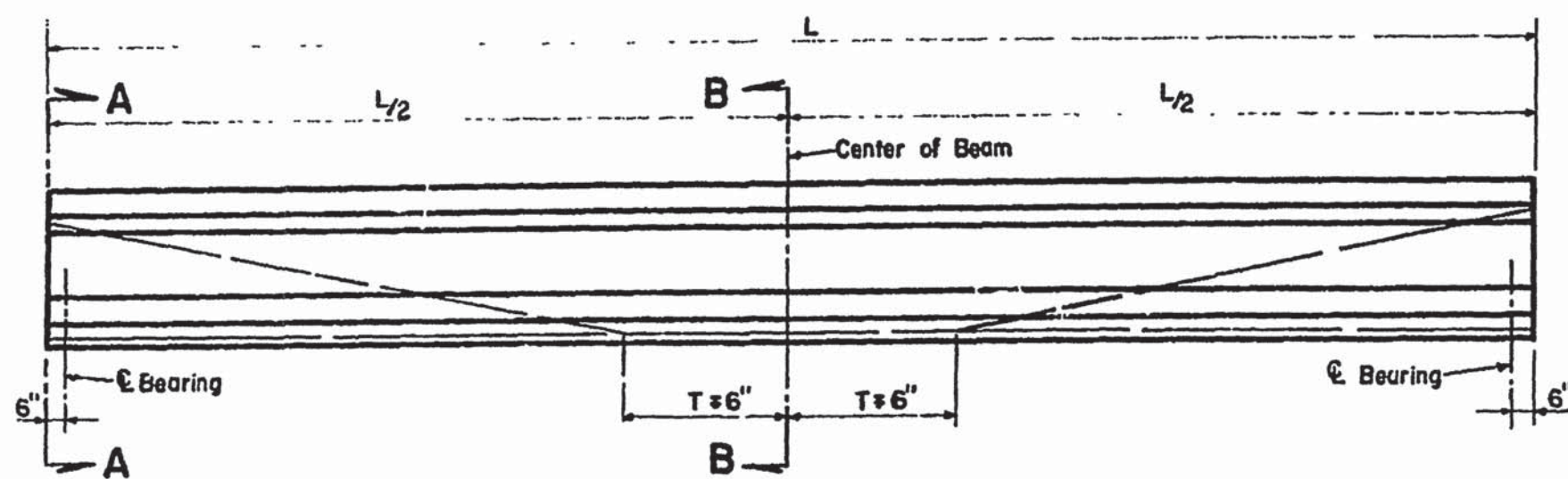
STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE
BENT DETAILS
LEFT AND RIGHT LANES
STATE ROUTE 34 OVER SOUTHERN
RAILROAD AND SNAPP FERRY ROAD
STATION 202+50.76
GREENE COUNTY
~1963~

DESIGNED BY: R. Green
DRAWN BY: W. D. Brinton Jr.
TRACED BY: R. Green, A. McCord
CHECKED BY: R. Green, A. McCord

DATE: 3-19-63
DATE: 4-2-63
DATE: 5-1-63

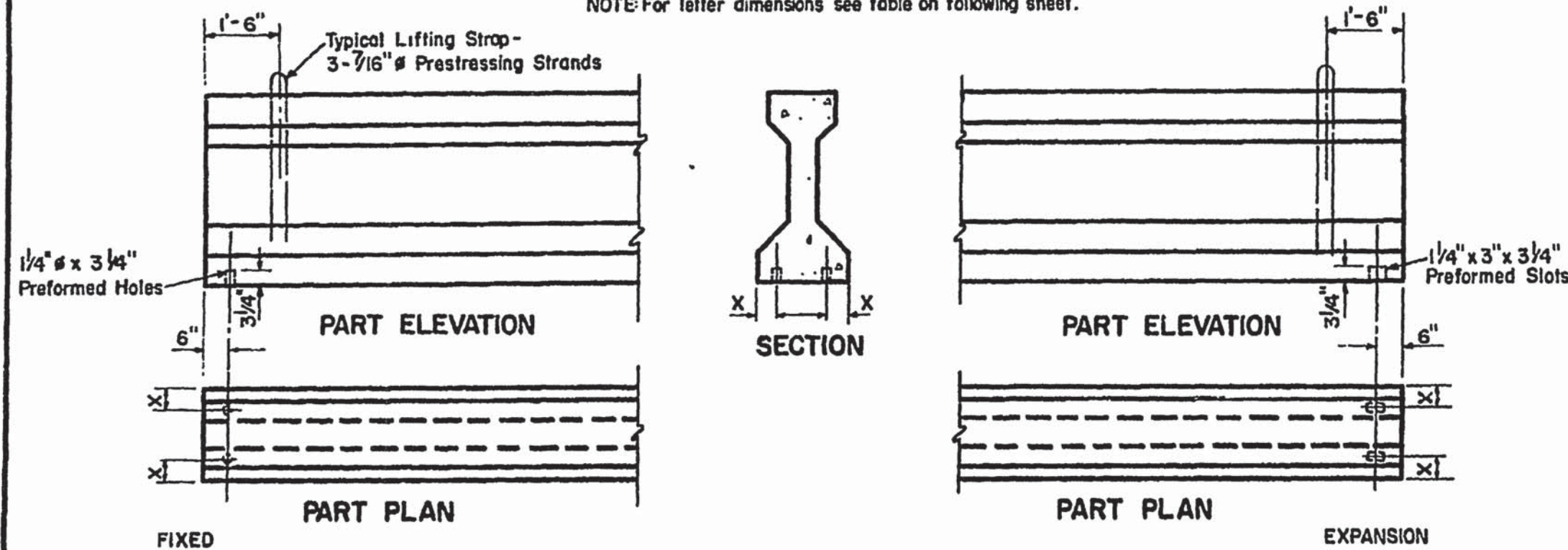
CORRECT: Fred Greve
BRIDGE ENGINEER
APPROVED: [Signature]
STATE HIGHWAY ENGINEER

NOTE: For Section A-A & B-B see Variable Prestressed Beam Detail sheet.

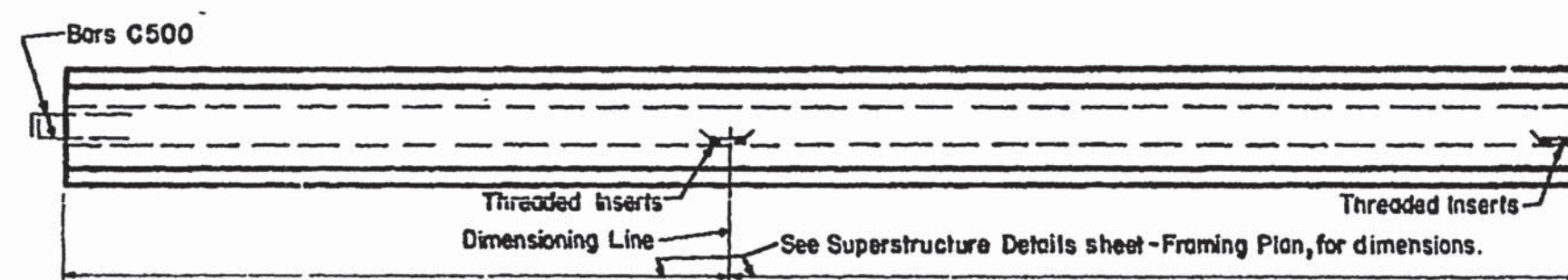


ELEVATION SHOWING STRAND DEFLECTION PATTERN

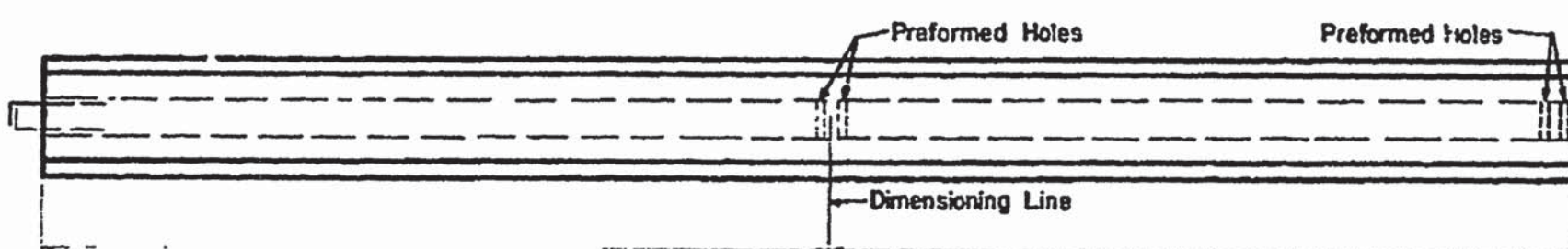
NOTE: For letter dimensions see table on following sheet.



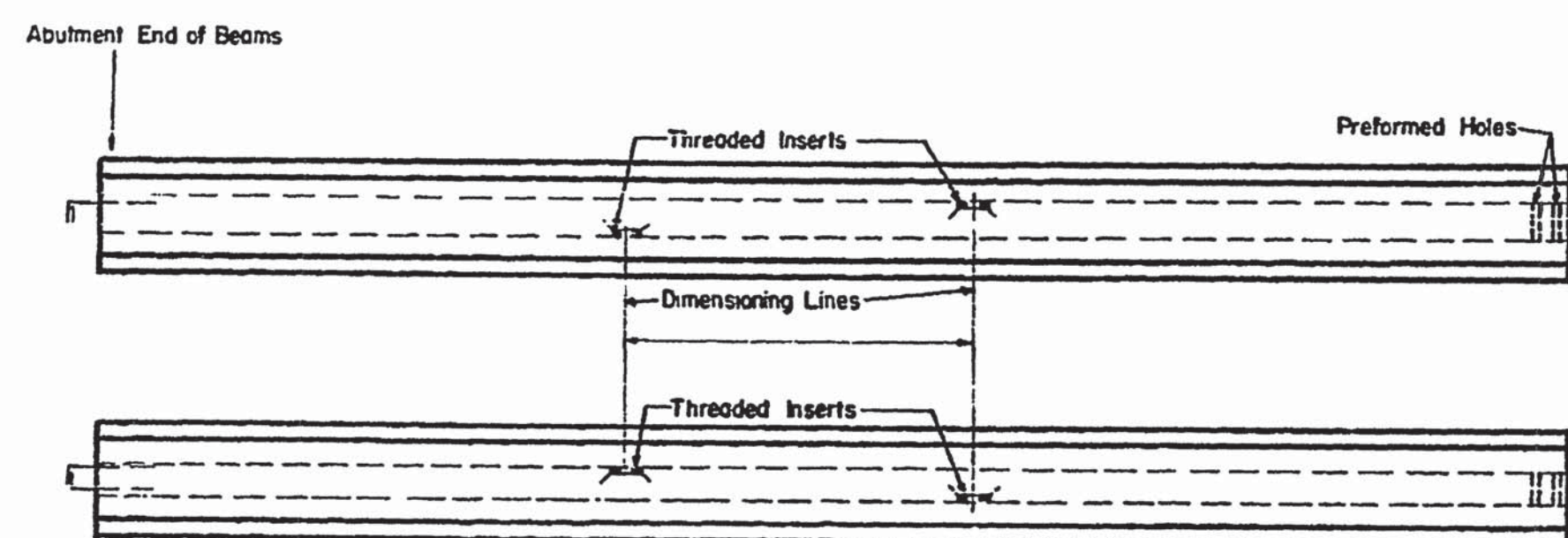
TYPICAL FIXED AND EXPANSION DETAILS AT BEARINGS



TYPICAL END SPAN-FASCIA BEAM



TYPICAL END SPAN-INTERIOR BEAM

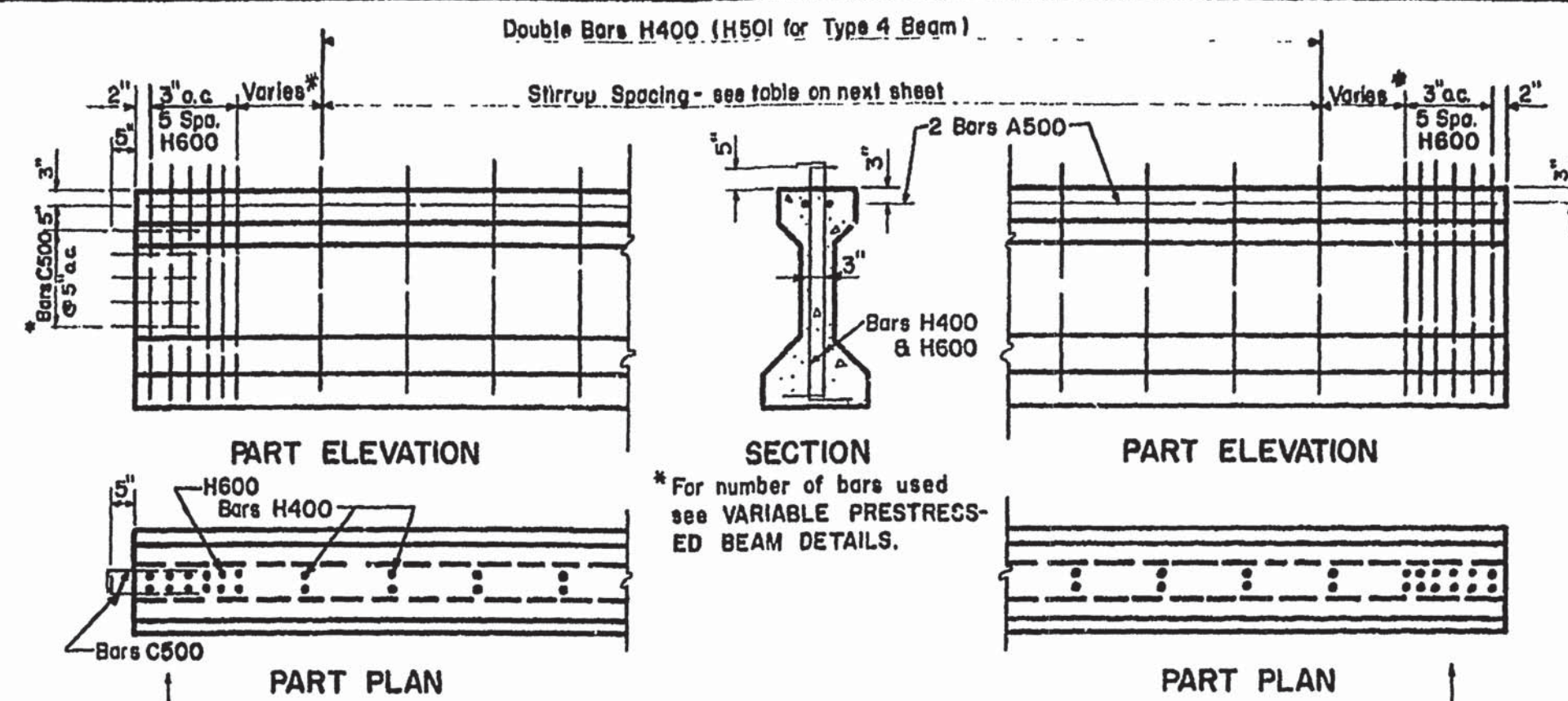


TYPICAL END SPAN-INTERIOR BEAM

NOTE: Skew controls location of Inserts.

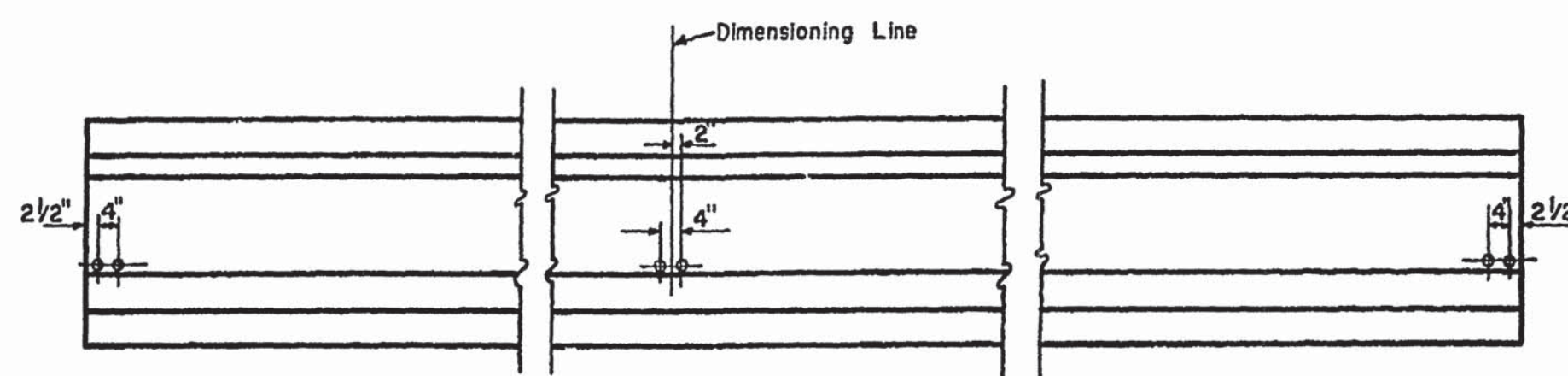
TYPICAL DIAPHRAGM CONNECTION DETAILS

NOTE: See Superstructure Details sheet for type used and location.



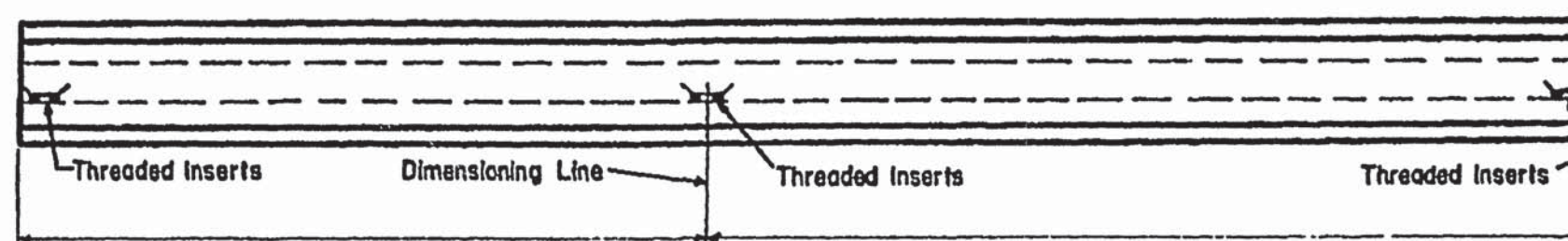
DETAILS SHOWING MILD STEEL REINFORCING

* Dimension To Be Equal To Or Less Than 1 Stirrup Space

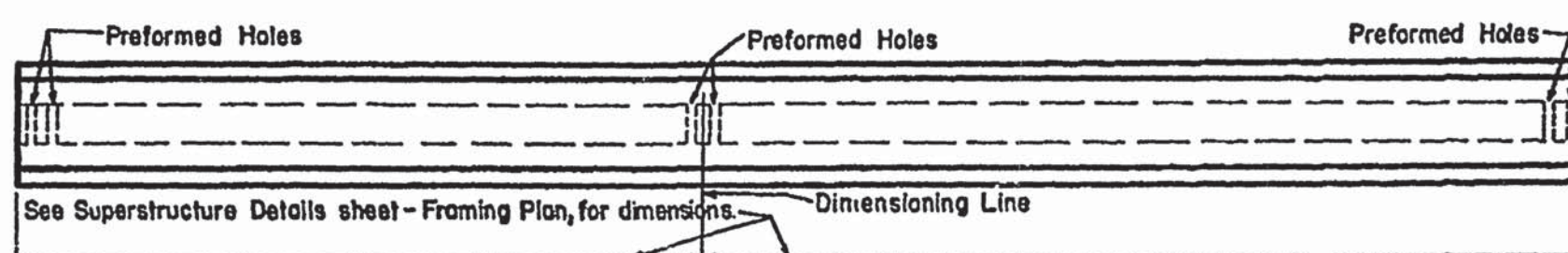


DETAILS OF DIAPHRAGM CONNECTIONS

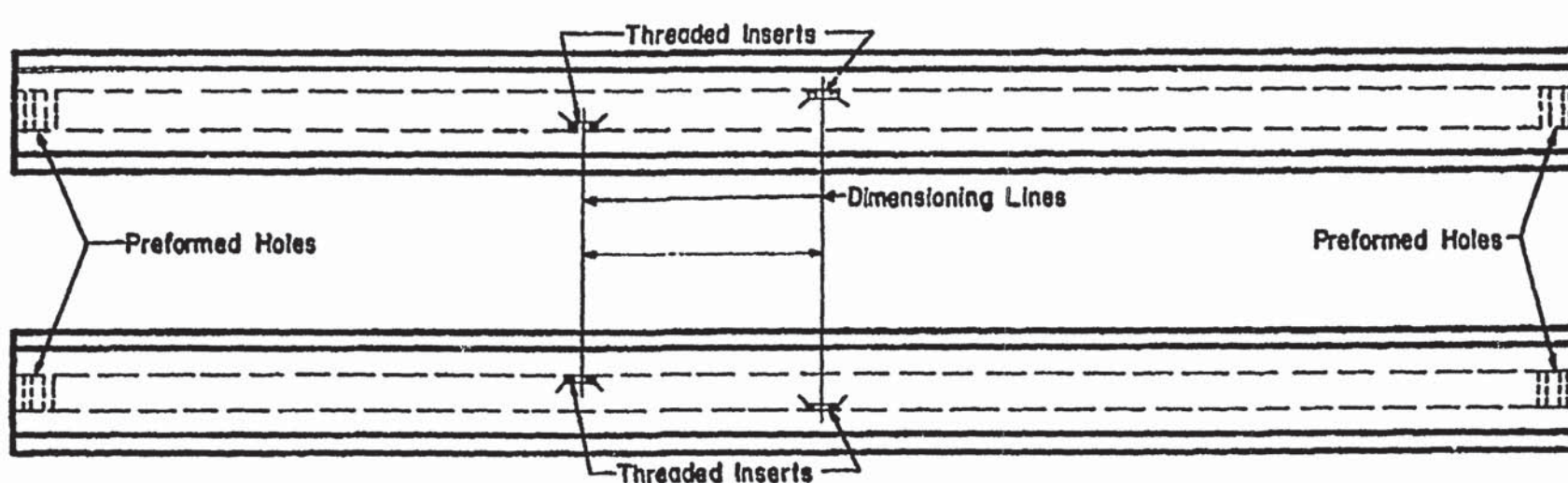
NOTE: For additional details and spacing of Preformed Holes and Threaded Inserts, see Superstructure Details sheet. See also details below.



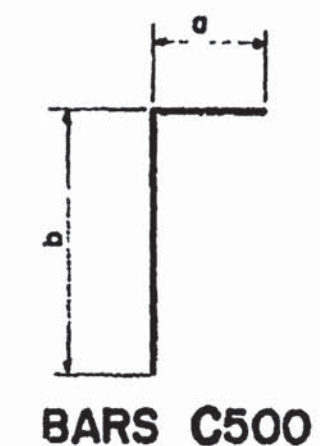
TYPICAL INTERMEDIATE SPAN-FASCIA BEAM



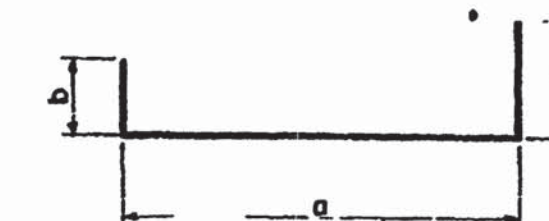
TYPICAL INTERMEDIATE SPAN-INTERIOR BEAM



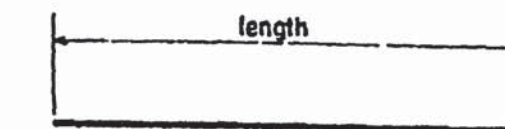
TYPICAL INTERMEDIATE SPAN-INTERIOR BEAM



BARS C500



BARS H400, H501 & H600



BARS A500

TYPE 1 BEAM						
Bar	Size	a	b	c	Length	
H400	4	2'-7"	6"	7"	3'-8"	
H600	6	2'-7"	6"	7"	3'-8"	
A500	5	See Beam Details for length				
C500	5	3"	1'-6"		1'-9"	

TYPE 2 BEAM						
Bar	Size	a	b	c	Length	
H400	4	3'-3"	6"	8"	4'-5"	
H600	6	3'-3"	6"	8"	4'-5"	
A500	5	See Beam Details for length				
C500	5	3"	1'-6"		1'-9"	

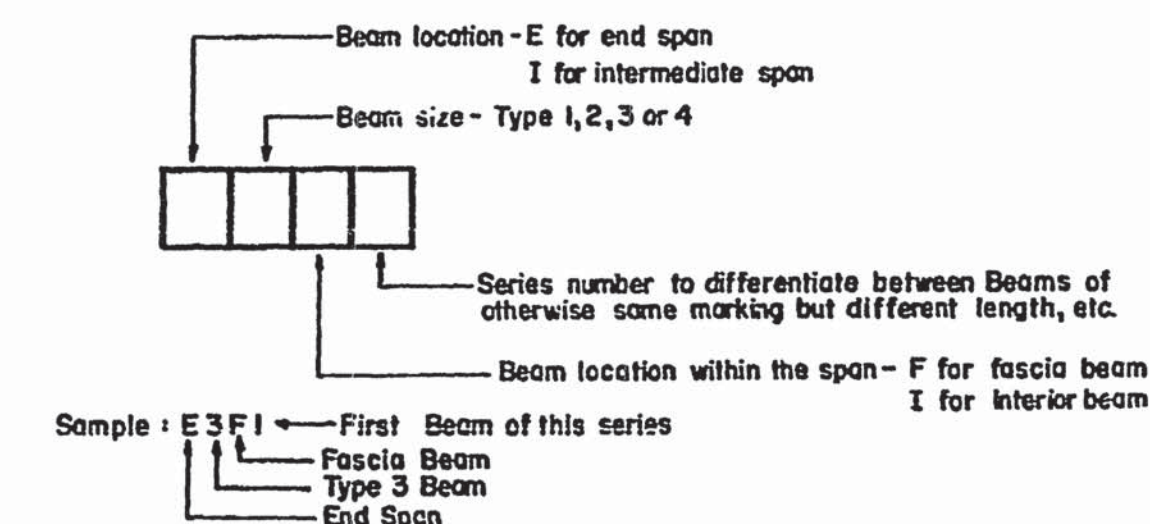
TYPE 3 BEAM						
Bar	Size	a	b	c	Length	
H400	4	4'-0"	6"	9"	5'-3"	
H600	6	4'-0"	6"	9"	5'-3"	
A500	5	See Beam Details for length				
C500	5	4"	1'-6"		1'-10"	

TYPE 4 BEAM						
Bar	Size	a	b	c	Length	
H501	5	4'-10"	6"	11"	6'-3"	
H600	6	4'-10"	6"	11"	6'-3"	
A500	5	See Beam Details for length				
C500	5	4"	1'-6"		1'-10"	

MILD STEEL REINFORCING BENDING DIMENSIONS

GENERAL NOTES

- The top of all Beams is to be rough floated. At approximately the time of initial set the top of Beams will also be scrubbed transversely with a coarse wire brush to remove all laitance and produce a rough surface.
- Preformed Holes for Diaphragm Connection shall be 1/4" #.
- Threaded Inserts for Diaphragm Connection are to be Richmond Type DTI or DTI-S or equal. Threaded Rods for Inserts are to provide a 2'-0" splice with Diaphragm Reinforcement. See Variable Prestressed Beam Detail sheet for number of Inserts and Threaded Rods.
- Anchorage at fixed and expansion ends of Beams accomplished by Dowels 1" # x 1'-6" long. See Variable Prestressed Beam Detail sheet for number of Dowel Bars.
- Mild Steel Reinforcing shall be Intermediate or Hard Grade.
- All Prestressing Strands to be 7 wire uncoated stress-relieved, size as noted in tables on following sheet.
- a. When using 1/2" # strands, an initial force of 25,200 lbs. shall be applied to each strand.
b. When using 7/16" # strands, an initial force of 18,900 lbs. shall be applied to each strand.
c. When using 3/8" # strands, an initial force of 14,000 lbs. shall be applied to each strand.



BEAM MARKING CODE EXPLANATION

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE
STANDARD PRESTRESSED BEAM DETAILS

Obsolete

DESIGNED BY: _____ DATE: _____
DRAWN BY: Joe L. Parks DATE: _____
TRACED BY: R. Hagar DATE: _____
CHECKED BY: _____ DATE: _____

CORRECT: _____
APPROVED: _____
K-15-60